



# Staff Report for PLCUP-2604-0009

## Application Information:

**Application Type:** Concurrent Use Permit

**Location of Subject:** 530 W Michigan St

**Legal Description:** 010-1230-00140

**Applicant:** MN Power

**Applicant Contact:** George Radosevich

**Agent:** Windsor Engineers

**Agent Contact:** Jack Dougherty

**Staff Contact:** Reina Owecke, [rowecke@duluthmn.gov](mailto:rowecke@duluthmn.gov)

### Deadline for Action:

**Application Date:** April 7, 2026

**Date Extension Letter Mailed:** April 15, 2026

**60 Days:** June 6, 2026

**120 Days:** August 5, 2026

**Site Visit Date:** April 29, 2026

**Sign Notice Date:** April 28, 2026

**Neighbor Letter Date:** n/a

**Number of Letters Sent:** n/a

## Proposal:

Applicant is seeking a concurrent use permit to install a new transformer and transition cabinet within the right-of-way between the existing parking structure at 602 W Michigan St and 530 W Michigan St.

## Recommended Action:

Staff recommends that Planning Commission recommend approval to the City Council.

## Zoning and Land Use:

	Current Zoning	Existing Land Use	Future Land Use Map Designation
<b>Subject</b>	F-8	Commercial	Central Business Primary
<b>North</b>	MU-C	Parking Lot	Central Business Primary
<b>South</b>	F-8	Parking Lot	Central Business Secondary
<b>East</b>	MU-C	Highway	Transportation/Utilities
<b>West</b>	F-8	Library	Central Business Primary

## Summary of Code Requirements:

**UDC Section 50-37.7.C.** The Planning Commission shall review the application, and Council shall approve the application or approve it with modifications, if it determines that:

1. The proposed concurrent use will not harm or inconvenience the health, safety and general welfare of the City;
2. Any proposed skywalk will significantly improve the circulation of pedestrians in the City without exposure to weather conditions;
3. No portion of a public easement proposed for use is being physically used or occupied by the public.

## Comprehensive Plan Governing Principle and/or Policies and Current History (if applicable):

### Future Land Use:

- Central Business Primary – Encompasses a broad range of uses and intensities: (1) Governmental campus, (2) Significant retail, (3) Entertainment and lodging, (4) Opportunities for high-density housing, (5) Central Plaza, public/open space, (6) Public parking facilities.

**History:** Not applicable.

## Review and Discussion Items:

### Staff finds:

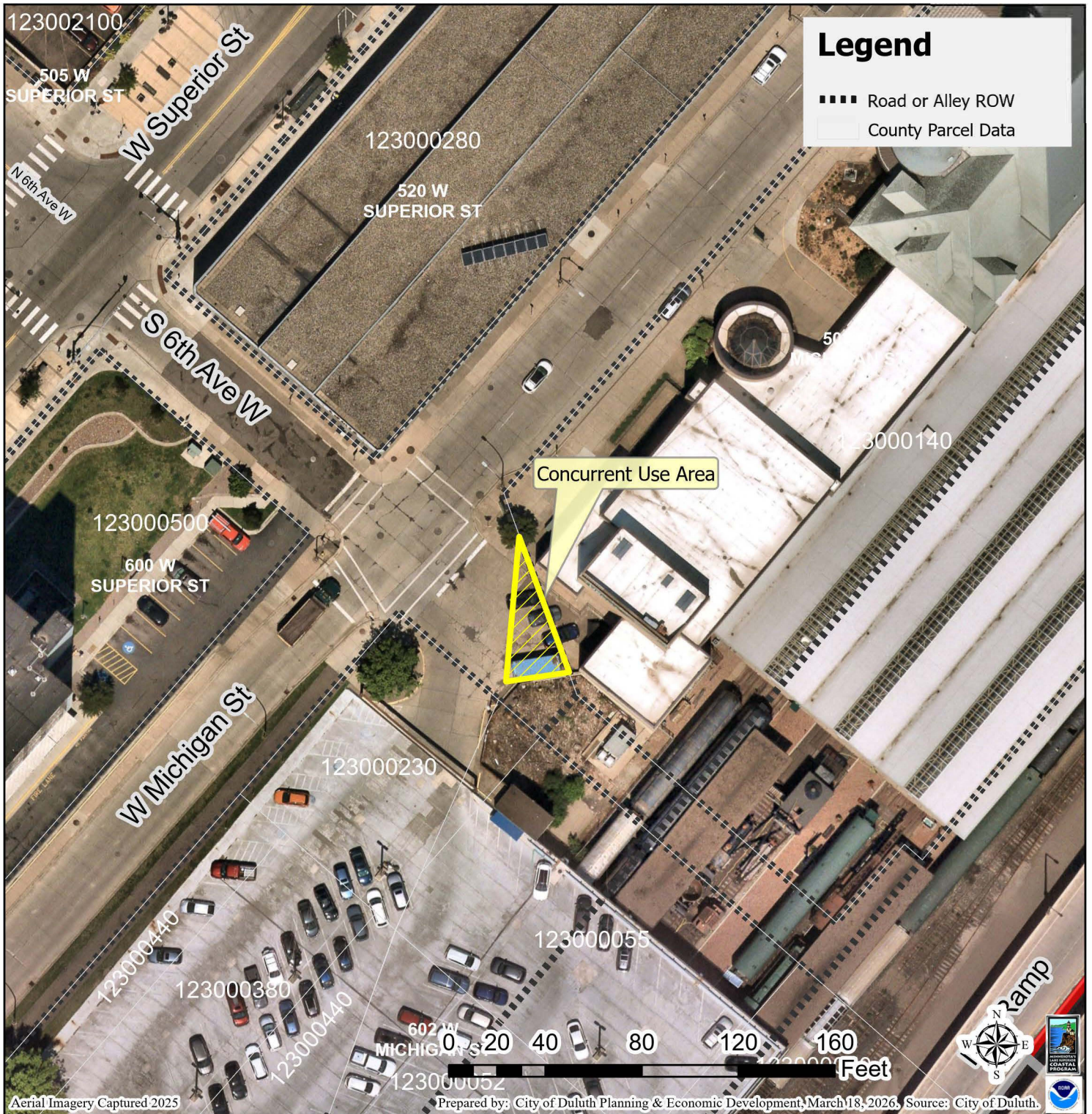
1. The applicant is seeking a concurrent use permit to install a new transformer and transition cabinet within the loading bay between the existing parking structure at 602 W Michigan St and the St. Louis County Depot at 530 W Michigan St. The proposed location of the new utilities will be within a new landscape island.
2. The Depot building is undergoing significant internal renovations which will improve the building's mechanical and electrical systems. Because of these renovations, a new transformer and transition cabinet are required to provide additional secondary service conduits.
3. New primary services from an existing transformer located along the Depot's western corner will be routed through the right-of-way to the proposed transformer. The existing transformer will remain.
4. The area proposed for the concurrent use permit will not take away from the public's ability to use the public right-of-way, nor will it harm or inconvenience the health, safety and general welfare of the City. The applicant will coordinate with the County and create new access to the parking structure at 602 W Michigan St during construction. Following construction, access to the parking structure will not be impacted.
5. Ownership of the new transformer will be with Minnesota Power. Liability, maintenance, and operation of the new utilities will be the responsibility of the applicant and Minnesota Power. In the event of an accident, the City will not be responsible for restoring or replacing any of the equipment.
6. Concurrent use permits are approved by the City Council via an ordinance. Unlike street right-of-way vacations, concurrent use permits are not recorded with St. Louis County as they are voidable agreements between the City and property owners.
7. The ordinance will require that the applicant locate any underground items in accordance with Gopher State One Call rules, and that the city will bear no responsibility for locating any facility related to the concurrent use permit except as it is legally responsible under the Gopher State One Call rules.
8. The City of Duluth requires concurrent use permits to include liability insurance to indemnify the City against all occurrences in the right-of-way. Other protections for the City and public may be included in any ordinance approving a concurrent use permit adopted by the City Council.
9. No City, agency, or public comments were received.
10. Per UDC Section 50-37.1.N, approved Concurrent Use Permits lapse if the project or activity authorized by the permit has not begun within one year.

## Staff Recommendation:

Based on the above findings, Staff recommends that Planning Commission recommend approval of the Concurrent Use Permit request to the City Council with the following conditions:

1. Applicant construct and maintain the project as identified in the attached exhibits.
2. Applicant provide annual proof of liability insurance and agree to indemnify the City against any occurrences in the right-of-way that are due to items covered under this permit.
3. Applicant will remove the obstructions at the applicant's expense if directed to by the City with appropriate notice.
4. Any alterations to the approved plans that do not alter major elements of the plan may be approved by the Land Use Supervisor without further Planning Commission approval; however, no such administrative approval shall constitute a variance from the provisions of Chapter 50.


# Staff Report Map for PLCUP-2604-0009:



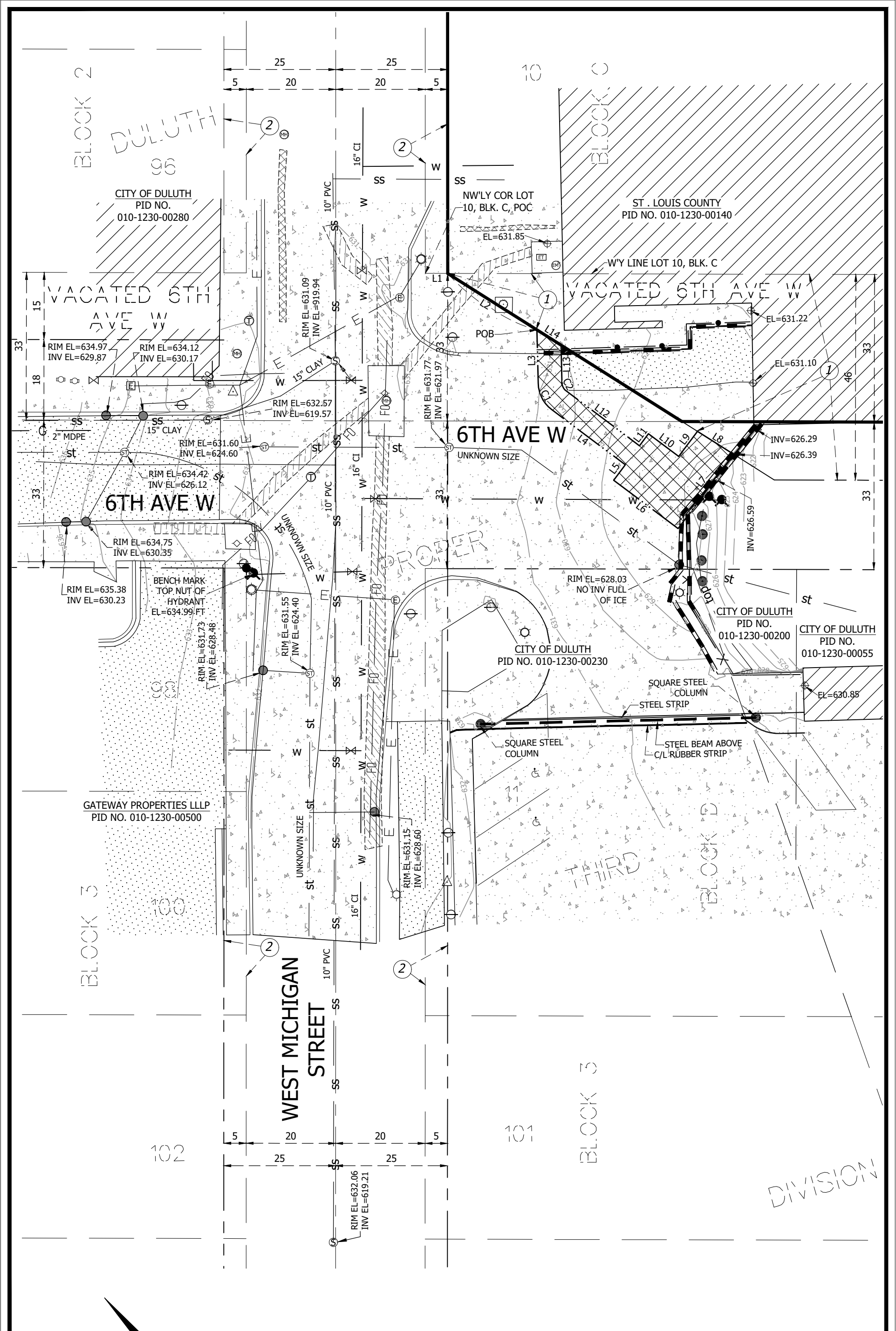
**Figure 1.**

Aerial imagery captured in 2025, showing a proposed Concurrent Use area near 530 W Michigan St.

The City of Duluth has tried to ensure that the information contained in this map or electronic document is accurate. The City of Duluth makes no warranty or guarantee concerning the accuracy or reliability. This drawing/data is neither a legally recorded map nor a survey and is not intended to be used as one. The drawing/data is a compilation of records, information and data located in various City, County and State offices and other sources affecting the area shown and is to be used for reference purposes only. The City of Duluth shall not be liable for errors contained within this data provided or for any damages in connection with the use of this information contained within.



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BLOCK 2

DULUTH  
96

CITY OF DULUTH  
PID NO.  
010-1230-00280

BLOCK C

ST. LOUIS COUNTY  
PID NO. 010-1230-00140

WY LINE LOT 10, BLK. C

VACATED 5TH AVE W

6TH AVE W

6TH AVE W

GATEWAY PROPERTIES LLLP  
PID NO. 010-1230-00500

CITY OF DULUTH  
PID NO. 010-1230-00230

CITY OF DULUTH  
PID NO. 010-1230-00200

CITY OF DULUTH  
PID NO. 010-1230-00055

WEST MICHIGAN  
STREET

BLOCK 3

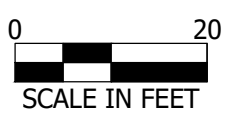
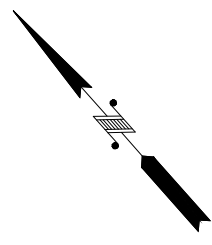
BLOCK 3

100

102

101

DIVISION



<b>BOUNDARY &amp; TOPOGRAPHIC SURVEY</b>		<p>ALTA LAND SURVEY COMPANY</p> <p>PHONE: 218-727-5211 LICENSED IN MN &amp; WI WWW.ALTLANDSURVEYDULUTH.COM</p>
CLIENT: ST. LOUIS COUNTY	REVISIONS: 4-3-26 ADD CONCURRENT USE AREA PER OTHERS	
ADDRESS: 506 W MICHIGAN STREET DULUTH, MN 55802		
DATE: 03-12-2026	JOB NO: 26-050 SHEET 2 OF 2	

LEGAL DESCRIPTION PER DOCUMENT NO. 169580

All that part of Lots 7, 8, 9 and 10, in Block C, Lot 11, in Block D, and Sixth Avenue West, DULUTH PROPER, THIRD DIVISION, according to the plat thereof on file and of record in the office of the Register of Deeds in and for said County and State, described as follows, to-wit:  
 Beginning at the intersection of the Southeasterly line of 50-foot-wide Michigan Street as now established and the Southwesterly line of Fifth Avenue West as now established; thence Southeasterly along said Southwesterly line of Fifth Avenue West a distance of 221 feet; thence Southwesterly along a straight line to a point on the Southwesterly line of said Lot 10, which point is 232 feet Southeasterly of the intersection of the Southwesterly line of said Lot 10 and said Southeasterly line of 50-foot wide Michigan Street; thence continue Southwesterly along a straight line to a point on the Northeasterly line of said Lot 11, which point is 233 feet Southeasterly of the intersection of the Northeasterly line of said Lot 11 and said Southeasterly line of 50-foot-wide Michigan Street;  
 thence continue Southwesterly along a straight line to a point on the Southwesterly line of said Lot 11, which point is 235.75 feet Southeasterly of the intersection of the Southwesterly line of said Lot 11 and said Southeasterly line of 50-foot-wide Michigan Street; thence Northwesterly along the Southwesterly line of said lot 11 a distance of 235.75 feet to the intersection of the Southwesterly line of said Lot 11 and said Southeasterly line of 50-foot-wide Michigan Street, thence Northeasterly along said Southeasterly line of 50-foot-wide Michigan Street to the intersection of said Southeasterly line of 50-foot-wide Michigan Street and the Northeasterly line of said Lot 11; thence continue Northeasterly along a straight line to the intersection of said Southeasterly line of 50-foot wide Michigan Street and the Southwesterly line of said Lot 10; thence continue Northeasterly along said Southeasterly line of 50-foot-wide Michigan Street a distance of 390 feet, more or less, to the point of beginning. Subject to all public easements in the above described part of Sixth Avenue West. Together with the streets and avenues appurtenant to said tract or parcel of land, subject to the public easements therein. Subject to the terms and conditions stated and the rights reserved by The Duluth Union Depot and Transfer Company, a Minnesota corporation, and Burlington Northern Inc., a Delaware corporation, in and by that certain deed to the above described premises executed by The Duluth Union Depot and Transfer Company and Burlington Northern Inc., as Grantors, to the party of the first part herein, as Grantee, dated November 9, 1972, and recorded in the office of the Register of Deeds, St. Louis County, Minnesota, on November 15; 1972, as Document No. 167436; and subject further to the rights of the Duluth Union Depot and Transfer Company and Burlington Northern Inc. under and by virtue of that certain agreement between The Duluth Union Depot and Transfer Company and Burlington Northern Inc., as parties of the first part and Area Cultural Center Corporation, as party of the second part, dated November 9, 1972, a copy of which is attached hereto as Exhibit A.

LEGAL DESCRIPTION OF CONCURRENT USE AREA

That part of Sixth Avenue West as dedicated on the plat of DULUTH PROPER THIRD DIVISION, according to the recorded plat thereof, St. Louis County, Minnesota described as follows:











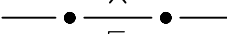


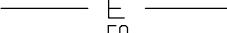




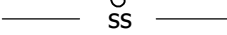

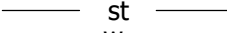

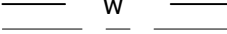

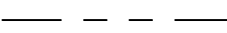





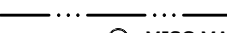















Commencing at the Northwesterly corner of Lot 10, Block C, said DULUTH PROPER THIRD DIVISION; thence on an assumed bearing of South 48 degrees 15 minutes 27 seconds East, along the Westerly line of said Lot 10 for a distance of 5.00 feet; thence South 16 degrees 04 minutes 57 seconds East 23.62 feet to the point of beginning of the parcel herein described; thence South 40 degrees 18 minutes 07 seconds West 11.92 feet; thence Southerly 6.93 feet, along a tangential curve, concave to the Northeast, said curve having a radius of 7.65 feet and a delta angle of 51 degrees 53 minutes 31 seconds; thence South 11 degrees 35 minutes 24 seconds East 20.49 feet; thence South 78 degrees 24 minutes 36 seconds West 4.35 feet; thence South 11 degrees 35 minutes 24 seconds East 18.00 feet; thence North 78 degrees 24 minutes 36 seconds East 20.73 feet; thence North 16 degrees 04 minutes 47 seconds West 9.58 feet; thence South 78 degrees 24 minutes 36 seconds West 6.98 feet; thence North 11 degrees 35 minutes 24 seconds West 8.45 feet; thence South 78 degrees 24 minutes 36 seconds West 3.35 feet; thence North 11 degrees 35 minutes 24 seconds West 20.49 feet; thence Northeasterly 2.13 feet, along a tangential curve, concave to the Northeast, said curve having a radius of 2.35 feet and a delta angle of 51 degrees 53 minutes 31 seconds; thence North 40 degrees 18 minutes 07 seconds East 8.40 feet; thence North 16 degrees 04 minutes 57 seconds West 6.36 feet to the point of beginning. Said parcel contains 491 square feet or 0.01 acres.

UNDERGROUND UTILITIES NOTES

- UTILITIES SHOWN HEREON, HAVE BEEN PREPARED, IN PART, ON THE BASIS OF INFORMATION COMPILED AND FURNISHED BY OTHERS. THE SURVEYOR, ENGINEER AND ARCHITECT WILL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS WHICH HAVE BEEN INCORPORATED INTO THIS DOCUMENT AS A RESULT.
- UNDERGROUND UTILITIES SHOWN ON THE SURVEY ARE BASED IN PART ON MARKINGS PROVIDED BY GOPHER STATE ONE CALL TICKET NUMBER 260560710.
- SOME UTILITIES SHOWN ON THIS SURVEY HAVE BEEN LOCATED BY MAPS AND OR UTILITY SKETCHES PROVIDED BY UTILITY COMPANIES, CITY OR COUNTY GIS. LOCATIONS ARE APPROXIMATE.
- UTILITIES SHOWN ON THIS SURVEY ARE SHOWN FOR APPROXIMATE LOCATION ONLY AND SHOULD BE VERIFIED BEFORE ANY EXCAVATION OR CONSTRUCTION.
- PRIVATE UTILITIES MAY EXIST BUT ARE NOT SHOWN ON THIS SURVEY. CONTACT A PRIVATE UTILITY LOCATOR FOR LOCATIONS.
- FIELD VERIFY SANITARY AND STORM SEWER PIPE SIZE AND LOCATION.

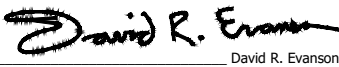

LINE	BEARING	DISTANCE	
L1	S48°15'27"E	5.00	
L2	S16°04'57"E	23.62	
L3	S40°18'07"W	11.92	
L4	S11°35'24"E	20.49	
L5	S78°24'36"W	4.35	
L6	S11°35'24"E	18.00	
L7	N78°24'36"E	20.73	
L8	N16°04'57"W	9.58	
L9	S78°24'36"W	6.98	
L10	N11°35'24"W	8.45	
L11	S78°24'36"W	3.35	
L12	N11°35'24"W	20.49	
L13	N40°18'07"E	8.40	
L14	N16°04'57"W	6.36	
CURVE	ARC LENGTH	RADIUS	DELTA ANGLE
C1	6.93	7.65	51°53'31"
C2	2.13	2.35	51°53'31"

LEGEND

	CONCRETE SURFACE		TOP OF BANK OR GRADE BREAK		HYDRANT
	BITUMINOUS SURFACE		CONCRETE CURB & GUTTER		WATER VALVE
	EXISTING BUILDINGS		RETAINING WALL		UTILITY VAULT
	FIBER OPTIC DUCT AS LOCATED		FENCE LINE		TELEPHONE MANHOLE
	ELECTRIC DUCT AS LOCATED		HAND RAILING LINE		LIGHT POLE
	CONCURRENT USE AREA		UNDERGROUND ELEC		SIGN
			UNDERGROUND FIBER OPTIC		DECIDUOUS TREE
			UNDERGROUND GAS		POST/BOLLARD
			SANITARY SEWER		SPOT ELEVATION
			STORM SEWER		ROOF DRAIN
			UNDERGROUND WATER LINE		HANDI CAPPED PARKING SPOT
			CENTER LINE		REFER TO SURVEYOR'S NOTES
			RIGHT OF WAY LINE		CONTROL POINT
			BOUNDARY LINE AS SURVEYED		FOUND MAG NAIL
			EXISTING PLAT LINE		ELEC PEDESTAL
			ADJACENT PARCEL LINE		ELEC MANHOLE
			CONCURRENT USE LINE		ELEC TRANSFORMER
			MISC MANHOLE		
			SANITARY MANHOLE		
			STORM MANHOLE		
			CATCH BASIN		
			POB-POINT OF BEGINNING		
			POC-POINT OF COMMENCEMENT		

SURVEYOR'S NOTES

- VACATION OF 6TH AVE W SOUTH OF MICHIGAN ST PER CITY RESOLUTION PASSED APRIL 14, 1975, ABSTRACT DOCUMENT NO. 213401.
- CONDEMNATION OF 5 FT EACH SIDE OF MICHIGAN STREET FROM 5TH AVE. WEST TO 8TH AVE. WEST PER BOOK 8, PAGE 351, 5-23-1887, CITY COUNCIL PROCEEDINGS PER CITY OF DULUTH ENGINEERING DEPARTMENT TRACING BOOK AND BOOK 1 OF CONDEMNATION PLATS, PAGES 38 & 39, 10-28-1887.
- THIS SURVEY HAS BEEN PREPARED WITHOUT BENEFIT OF A TITLE COMMITMENT OR TITLE OPINION. A TITLE SEARCH FOR RECORDED OR UNRECORDED EASEMENTS WHICH MAY BENEFIT OR ENCUMBER THIS PROPERTY HAS NOT BEEN COMPLETED BY ALTA LAND SURVEY COMPANY. THE SURVEYOR ASSUMES NO RESPONSIBILITY FOR SHOWING THE LOCATION OF RECORDED OR UNRECORDED EASEMENTS OR OTHER ENCUMBRANCES NOT PROVIDED TO THE SURVEYOR AS OF THE DATE OF THE SURVEY.
- BEARINGS ARE BASED ON THE ST. LOUIS COUNTY TRANSVERSE MERCATOR COORDINATE SYSTEM OF 1996. (NAD 83 2011)
- NO SPECIFIC SOILS INVESTIGATION HAS BEEN COMPLETED ON THIS LOT BY ALTA LAND SURVEY COMPANY. THE SUITABILITY OF SOILS TO SUPPORT THE SPECIFIC STRUCTURE PROPOSED IS NOT THE RESPONSIBILITY OF ALTA LAND SURVEY COMPANY OR THE SURVEYOR.
- BENCHMARK SHOWN ON SURVEY.
- DUE TO SNOW AND ICE COVERAGE, SOME FEATURES MAY NOT HAVE BEEN VISIBLE DURING THE COURSE OF THIS SURVEY.

I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.  David R. Evanson MN License #49505 DATE:03-12-2026	<b>BOUNDARY &amp; TOPOGRAPHIC SURVEY</b>		 <b>ALTA</b> LAND SURVEY COMPANY PHONE: 218-727-5211 LICENSED IN MN & WI WWW. ALTALANDSURVEYDULUTH.COM
	CLIENT: ST. LOUIS COUNTY	REVISIONS: 4-3-26 ADD CONCURRENT USE AREA PER OTHERS	
ADDRESS: 506 W MICHIGAN STREET DULUTH, MN 55802	DATE: 03-12-2026	JOB NO: 26-050	SHEET 1 OF 2

## PROJECT NOTES

PROJECT INCLUDES ST. LOUIS COUNTY DEPOT

PARCEL NO.(S): 010-1230-00140 AND 010-1230-00230

SITE ADDRESS: 506 W MICHIGAN ST, DULUTH, MN 55802

ZONING: F-8

LEGAL DESCRIPTION: LOT 10 OF BLOCK C OF DULUTH PROPER THIRD DIVISION AND LOT 11 OF BLOCK D OF DULUT PROPER THIRD DIVISION

COUNTY: ST LOUIS

### CRITICAL AREAS:

ON THE SUBJECT PARCEL, THERE ARE NO KNOWN: WETLANDS ON SITE SEPTIC TANKS DRAIN FIELDS, EXISTING / PROPOSED WELLS

## GOVERNING SPECIFICATIONS

THE 2019 EDITION OF THE CITY OF DULUTH STANDARD CONSTRUCTION SPECIFICATIONS AND SUPPLEMENTS OR ADDENDUMS SHALL APPLY. AVAILABLE AT: GOVERNMENTAL SERVICES BUILDING <https://duluthmn.gov/engineering/standard-construction-specifications>

THE 2022 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR CONSTRUCTION SHALL GOVERN. AVAILABLE AT: <http://www.dot.state.mn.us/pre-letting/spec/index.html>

## CONTACT INFORMATION

### APPLICANT

ARCHITECT ADVANTAGE  
2715 PIEDMONT AVENUE  
DULUTH, MN 55811  
(218) 724-5568  
CONTACT: SHANE NIES

### REPRESENTATIVE / CONTACT

WINDSOR ENGINEERS, LLC  
4802 ONEOTA STREET, SUITE 2  
DULUTH, MN 55807

CONTACT:  
JOHN DOUGHERTY  
(218) 203-8323  
JD@DOUGHERTY@WINDSORENGINEERS.COM

### SURVEYOR

ALTA LAND SURVEY COMPANY  
102 S. 21ST AVE. W #4  
DULUTH, MN 55806  
DAVID EVANSON  
(218) 727-5211

## GENERAL PLAN NOTES

1. CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION. A MINIMUM OF TWO FULL BUSINESS DAYS PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL CALL 811 (UTILITY NOTIFICATION CENTER) FOR LOCATION MARK-UP OF EXISTING UTILITIES

3. IN CASE OF A CONFLICT BETWEEN THE REGULATORY STANDARDS OR SPECIFICATIONS, THE MORE STRINGENT REQUIREMENT WILL PREVAIL.

4. ANY CHANGES TO THE DESIGN AND/OR CONSTRUCTION SHALL BE APPROVED BY THE OWNER OR ENGINEER.

5. APPROVAL OF THESE PLANS DOES NOT CONSTITUTE AN APPROVAL OF ANY OTHER CONSTRUCTION NOT SPECIFICALLY SHOWN ON THE PLANS. PLANS FOR STRUCTURES SUCH AS BRIDGES, BUILDINGS, TANKS, VAULTS, ROCKERIES, AND RETAINING WALLS MAY REQUIRE A SEPARATE REVIEW AND APPROVAL BY THE BUILDING DEPARTMENT PRIOR TO CONSTRUCTION.

6. A COPY OF THESE APPROVED PLANS SHALL BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.

7. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN ALL CONSTRUCTION EASEMENTS AND PERMITS NECESSARY TO PERFORM THE WORK.

8. THE CONTRACTOR IS RESPONSIBLE FOR ALL CONSTRUCTION STAKING.

9. PUBLIC AND PRIVATE DRAINAGE WAYS SHALL BE PROTECTED FROM POLLUTION. NO MATERIAL IS TO BE DISCHARGED TO OR DEPOSITED IN STORMWATER SYSTEMS THAT MAY RESULT IN VIOLATION OF STATE OR FEDERAL WATER QUALITY STANDARDS.

10. ALL CONSTRUCTION WITHIN THE PUBLIC RIGHT-OF-WAY SHALL HAVE AN APPROVED PUBLIC RIGHT-OF-WAY WORK PERMIT PRIOR TO ANY CONSTRUCTION ACTIVITY WITHIN THE RIGHT-OF-WAY.

11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ADEQUATE SAFEGUARDS, SAFETY DEVICES, PROTECTIVE EQUIPMENT, FLAGGERS, AND ANY OTHER NEEDED ACTIONS TO PROTECT THE LIFE, HEALTH, AND SAFETY OF THE PUBLIC, COMPLY WITH OSHA REGULATIONS, AND TO PROTECT PROPERTY IN CONNECTION WITH THE PERFORMANCE OF WORK COVERED BY THE CONTRACTOR. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST ADOPTED EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) PUBLISHED BY THE U.S. DEPARTMENT OF TRANSPORTATION. TWO-WAY TRAFFIC MUST BE MAINTAINED AT ALL TIMES ON THE ADJACENT PUBLIC STREETS.

12. ANY PUBLIC OR PRIVATE CURB, GUTTER, SIDEWALK, OR ASPHALT DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO CITY/COUNTY STANDARDS AND PRACTICES.

13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE INTEGRITY OF ADJACENT UTILITIES WHICH MAY INCLUDE, BUT ARE NOT LIMITED TO, WATER, SANITARY SEWER, STORMWATER, POWER, TELEPHONE, CABLE TV, GAS, IRRIGATION, AND STREET LIGHTING. THE CONTRACTOR SHALL NOTIFY RESIDENTS AND BUSINESSES 48 HOURS IN ADVANCE OF ANY WORK AFFECTING ACCESS OR SERVICE AND SHALL MINIMIZE INTERRUPTIONS TO DRIVEWAYS FOR RESIDENTS AND BUSINESSES ADJACENT TO THE PROJECT.

14. ALL LAWN AND VEGETATED AREAS DISTURBED WILL BE RESTORED TO ORIGINAL CONDITION. ANY DISTURBANCE OR DAMAGE TO OTHER PROPERTY ON ADJACENT PARCELS OR IN THE PUBLIC RIGHT OF WAY SHALL ALSO BE REPAIRED OR RESTORED TO ORIGINAL CONDITION.

15. ALL MATERIALS AND METHODS OF CONSTRUCTION AND INSTALLATION FOR WATER, SANITARY SEWER, AND STORM FACILITIES SHALL CONFORM TO THE AHJ AND LGU. CONSTRUCTION SHALL BE AS PER THE MOST CURRENT STANDARD DETAIL CONTAINED THEREIN.

16. THE TYPES, LOCATIONS, SIZES AND/OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AND STRUCTURES AS SHOWN ON THE DRAWINGS ARE APPROXIMATE AND WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY. THE CONTRACTOR IS CAUTIONED THAT ONLY ACTUAL EXCAVATION WILL REVEAL THE TYPES, EXTENT, SIZES, LOCATIONS, AND DEPTHS OF UTILITIES. A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE ALL KNOWN UNDERGROUND UTILITIES. THE CONTRACTOR SHALL VERIFY THE LOCATION OF AND PROVIDE PROTECTION FOR ALL UTILITIES AND STRUCTURES. OVERHEAD UTILITIES ARE NOT SHOWN.

17. EXISTING UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR OR BY THE UTILITY.

18. WHERE THE CONTRACTOR MUST RELOCATE WATER AND GAS UTILITIES, SHUTDOWN SHALL ONLY BE ACCOMPLISHED BY THE CITY OR UTILITY PURVEYOR.

19. ALL OPEN TRENCHES THAT IMPACT PUBLIC ACCESS OR OTHER PROJECT WORK ACCESS OUTSIDE OF THIS PROJECT'S SITE, MUST BE STEEL PLATED OR BACKFILLED AND PAVED WITH AT LEAST 2" OF COLD MIX TO ADJACENT EXISTING GRADE AT THE END OF EACH WORKDAY.

20. NOTIFY ADJACENT RESIDENCES AT LEAST ONE DAY PRIOR TO COMMENCING WORK ADJACENT TO THEIR RESIDENCES.

21. SAWCUT ALL PAVEMENT JOIN LINES. WHERE THERE IS A PREVIOUS PAVING EDGE OR CRACK WITHIN 5' OF THE SAWCUT EDGE, REMOVE THE PAVEMENT TO THE PREVIOUS PAVING EDGE.

22. THE CONTRACTOR SHALL COMPLY WITH MINNESOTA REQUIREMENTS FOR TRENCH SAFETY.

23. THE CONTRACTOR SHALL REPLACE ALL SURVEY MONUMENTS THAT ARE DESTROYED BY THE CONSTRUCTION.

24. ALL WATER SERVICE PIPING SHALL BE CONSTRUCTED WITH 7" MINIMUM COVER. ALL WATER MAIN PIPING SHALL BE CONSTRUCTED WITH 7.5' MINIMUM COVER. 1' VERTICAL SEPARATION BETWEEN UTILITIES, AND A MINIMUM OF 10' HORIZONTAL SEPARATION AND 18" ABOVE SEWER LINES, UNLESS OTHERWISE NOTED.

25. THE CONTRACTOR SHALL RESTORE PAVEMENT AND LANDSCAPING DISTURBED BY THE CONSTRUCTION TO THE PREVIOUSLY UNDISTURBED CONDITION.

26. CONTRACTOR TO DISPOSE OF TREES, SHRUBS, SOD AND OTHER DEBRIS IN A PROPER MANNER OF THE CONTRACTOR'S CHOOSING.

27. CONTRACTOR IS RESPONSIBLE FOR KEEPING ALL ROADS, SIDEWALK, AND TRAILS CLEAN AND CLEAR FROM CONSTRUCTION MATERIAL AND DEBRIS.

28. IN ADDITION TO IN-PLACE DENSITY TESTING, THE SUBGRADE AND BASE ROCK SHALL BE PROOF-ROLLED WITH A LOADED 10-YD DUMP TRUCK PROVIDED BY THE CONTRACTOR. BASE ROCK PROOF-ROLL SHALL TAKE PLACE IMMEDIATELY PRIOR TO (WITHIN 24 HOURS OF) PAVING, AND SHALL BE WITNESSED BY THE OWNERS REPRESENTATIVE. LOCATION AND PATTERN OF PROOF-ROLL TO BE AS DIRECTED BY OWNERS REPRESENTATIVE OR APPROVING AGENCY.

## GENERAL CIVIL NOTES

SURVEY  
BOUNDARY SURVEY: ALTA LAND SURVEY COMPANY  
TOPOGRAPHIC: SURFACE FROM ALTA TOP

HORIZONTAL DATUM: SAINT LOUIS COUNTY  
VERTICAL DATUM: NAVD88  
COORDINATE SYSTEM: SAINT LOUIS COUNTY TRANSVERSE MERCATOR 96

STORM DRAINAGE:  
ON-SITE STORM SEWER IMPROVEMENTS SHALL CONFORM TO THE LATEST VERSION OF THE CITY OF DULUTH STANDARD SPECIFICATIONS. IN INSTANCES WHERE CITY OF DULUTH STANDARD SPECIFICATIONS DO NOT APPLY, MNDOT SPECIFICATIONS SHOULD BE FOLLOWED.

THE CONTRACTOR SHALL MAINTAIN 6" MINIMUM VERTICAL AND 3' MINIMUM HORIZONTAL CLEARANCE (OUTSIDE SURFACES) BETWEEN STORM DRAIN PIPES AND OTHER UTILITY PIPES AND CONDUITS. FOR CROSSINGS OF SANITARY SEWER LINES, THE MINNESOTA POLLUTION CONTROL AGENCY CRITERIA APPLY.

STORM DRAIN PIPE, BENDS, AND FITTINGS SHALL BE PVC, ASTM D 3034, SDR 35, OR SMOOTH INTERIOR, HIGH DENSITY POLYETHYLENE CORRUGATED PIPE AASHTO M252 OR M254, TYPE S AS PRODUCED AND SPECIFIED BY ADS. PRODUCT NAME N12, OR APPROVED EQUAL. ALL STORM SEWER FITTINGS AND PIPE JOINTS SHALL BE GASKETED.

ALL STORM SEWER PIPE SHALL HAVE A MINIMUM 12" DIAMETER WITHIN ROADWAY

ALL ON-SITE STORMWATER FACILITIES SHALL BE PRIVATELY MAINTAINED BY THE CURRENT OR FUTURE PROPERTY OWNER(S).

ALL VAULT, UTILITY BOX, INLET, MANHOLE AND CLEANOUT RIMS SHALL BE ADJUSTED TO FINISH GRADE UNLESS OTHERWISE NOTED.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT AND MAINTAIN ANY STORM SYSTEM PIPING TO EXISTING DRAINAGE APPURTENANCES TO REMAIN.

CONTRACTOR IS RESPONSIBLE FOR ALL PERMITS AND INSPECTIONS.

### ARCHAEOLOGY

IF ANY CULTURAL RESOURCES AND/OR HUMAN REMAINS ARE DISCOVERED IN THE COURSE OF UNDERTAKING THE DEVELOPMENT ACTIVITY, THE DEPARTMENT OF ARCHAEOLOGY AND HISTORIC PRESERVATION SHALL BE NOTIFIED. FAILURE TO COMPLY WITH THESE STATE REQUIREMENTS MAY CONSTITUTE A CLASS C FELONY, SUBJECT TO IMPRISONMENT AND/OR FINES.

## GRADING & EROSION CONTROL NOTES

FINISH GRADE CONTOURS ARE TO TOP OF FINISHED SURFACE IN IMPERVIOUS AREAS AND TOP OF REPLACED STRIPPINGS IN PERVIOUS AREAS.

CUT AND FILL QUANTITIES HAVE NOT BEEN DETERMINED. CONTRACTOR TO PERFORM QUANTITY TAKEOFF FOR ESTIMATING PURPOSES. CONTRACTOR SHALL BE RESPONSIBLE FOR HAUL-OFF EXCESS MATERIAL.

## AMERICANS WITH DISABILITIES ACT (ADA) NOTES

1. CONTRACTORS SHALL EXERCISE APPROPRIATE CARE AND PRECISION IN CONSTRUCTION OF ADA ACCESSIBLE COMPONENTS ON THE PROJECT, THE ADA COMPONENTS MUST COMPLY WITH ALL LOCAL, STATE, AND FEDERAL ACCESSIBILITY RULES, CODES, AND REGULATIONS.

2. FINISHED SURFACES ALONG THE ACCESSIBLE PATH OF TRAVEL FROM PARKING STALLS, PUBLIC TRANSPORTATION, AND PEDESTRIAN ACCESS WAYS TO THE POINT(S) OF ACCESSIBLE BUILDING INGRESS AND EGRESS SHALL COMPLY WITH ADA CODE REQUIREMENTS.

3. PARKING SPACE AND AISLE SLOPE SHALL NOT EXCEED 1:48 (1/4" PER FOOT OR NOMINALLY 2.0%) IN ANY DIRECTION.

4. CURB RAMP SLOPE SHALL NOT EXCEED 1:12 (8.3%) AND RAMP LENGTH IS LIMITED TO 15 FEET.

5. LANDINGS SHALL BE PROVIDED AT EACH END OF RAMPS, SHALL HAVE POSITIVE DRAINAGE, AND SHALL NOT EXCEED 1:48 (1/4" PER FOOT OR NOMINALLY 2.0%) IN ANY DIRECTION.

6. PATH OF TRAVEL ALONG ACCESSIBLE ROUTE SHALL PROVIDE A MINIMUM OF 36 INCH UNOBSTRUCTED WIDTH OF TRAVEL. SLOPE SHALL BE NO GREATER THAN 1:20 (5.0% OR 5/8" PER FOOT) IN THE DIRECTION OF TRAVEL, AND SHALL NOT EXCEED 1:48 (1/4" PER FOOT OR NOMINALLY 2.0%) IN CROSS SLOPE. WHERE PATH OF TRAVEL BE GREATER THAN 1:20 (5.0%), AN ACCESSIBLE RAMP WITH A MAXIMUM SLOPE OF 1:12 (8.3%) FOR A MAXIMUM DISTANCE OF 30 FEET SHALL BE PROVIDED INCLUDING HANDRAILS. THE RAMP SHALL HAVE ACCESSIBLE HAND RAILS AND LANDINGS ON EACH END WITH A SLOPE IN ANY DIRECTION NOT EXCEEDING 1:48 (1/4" PER FOOT OR NOMINALLY 2.0%).

7. DOORWAYS SHALL HAVE A LANDING AREA ON THE EXTERIOR SIDE OF THE DOOR THAT IS SLOPED NO MORE THAN 1:48 (1/4" PER FOOT OR NOMINALLY 2.0%) FOR POSITIVE DRAINAGE. THIS LANDING AREA SHALL BE NO LESS THAN 60 INCHES (5 FEET) LONG, EXCEPT WHERE OTHERWISE PERMITTED BY ACCESSIBILITY STANDARDS FOR ALTERNATIVE DOORWAY OPENING CONDITIONS AND APPROVED BY THE OWNER'S REPRESENTATIVE.

8. WHERE PEDESTRIAN ACCESS ROUTES ARE CONTAINED WITHIN A STREET OR HIGHWAY RIGHT-OF-WAY, THE GRADE OF THE PEDESTRIAN ACCESS ROUTE IS PERMITTED TO EQUAL THE GENERAL GRADE ESTABLISHED FOR THE ADJACENT STREET OR HIGHWAY, EXCEPT THAT WHERE PEDESTRIAN ACCESS ROUTES ARE CONTAINED WITHIN PEDESTRIAN STREET CROSSINGS A MAXIMUM GRADE OF 5 PERCENT IS REQUIRED. (EXCERPT FROM PROWAG)

## STANDARD NOTES FOR EROSION CONTROL PLAN

1. APPROVAL OF THIS EROSION/SEDIMENTATION CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G. SIZE AND LOCATION OF ROADS, PIPES, RESTRICTIONS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.)

2. THE IMPLEMENTATIONS OF THESE ESC PLANS AND THE CONSTRUCTIONS, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/CONTRACTOR, UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND VEGETATION/LANDSCAPING IS ESTABLISHED.

3. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE APPLICANT/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.

4. THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DO NOT ENTER THE DRAINAGE SYSTEMS, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.

5. THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DO NOT LEAVE THE SITE.

6. THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.

7. THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN THE 48 HOURS FOLLOWING A STORM EVENT.

8. AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATIONS SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.

9. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES MAY BE REQUIRED TO ENSURE ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.

10. THE PROPOSED STORMWATER FACILITY MUST REMAIN OFFLINE UNTIL THE PROJECT IS COMPLETE AND VEGETATION THROUGHOUT THE SITE HAS BEEN ESTABLISHED. IF AT ANY TIME THE FACILITY BECOMES NUNDATED WITH SEDIMENT FILLED RUNOFF, THE TOP 2" OF MATERIAL MUST BE REMOVED AND REPLACED PRIOR TO PROJECT COMPLETION.

## RECOMMENDED CONSTRUCTION SEQUENCE FOR EROSION CONTROL

1. PRE-CONSTRUCTION MEETING.

2. FLAG OR FENCE CLEARING LIMITS

3. POST NOTICE OF CONSTRUCTION ACTIVITY SIGN WITH THE NAME AND PHONE NUMBER OF EROSION SEDIMENT CONTROL SUPERVISOR.

4. INSTALL CATCH BASIN PROTECTION, IF REQUIRED.

5. INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC.).

6. INSTALL TREE PROTECTION FENCING.7. GRADE AND INSTALL CONSTRUCTION ENTRANCE(S).

8. CONSTRUCT SEDIMENT POND(S) AND TRAP(S).

9. GRADE AND STABILIZE CONSTRUCTION ROADS.

10. CONSTRUCT SURFACE WATER CONTROLS (INTERCEPT DIKES, PIPE SLOPE DRAINS, ETC) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.

11. MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH STORMWATER MANAGEMENT MANUAL FOR WESTERN MINNESOTA AND MANUFACTURER'S RECOMMENDATIONS

12. RELOCATE SURFACE WATER CONTROLS AND EROSION CONTROL MEASURE OR INSTALL NEW MEASURES SO THAT AS SITE CONDITIONS CHANGE THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE CLARK COUNTY EROSION AND SEDIMENT CONTROL STANDARDS.

13. COVER ALL AREAS THAT WILL BE UNWORKED FOR MORE THAN SEVEN DAYS DURING THE DRY SEASON OR TWO DAYS DURING THE WET SEASON WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING OR EQUIVALENT.

14. STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN SEVEN DAYS.

15. SEED OR SOD ANY AREAS TO REMAIN UNWORKED FOR MORE THAN 30 DAYS.

16. UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BEST MANAGEMENT PRACTICES REMOVED IF APPROPRIATE.

## GENERAL FIRE NOTES

1. GENERAL FIRE SAFETY PRECAUTIONS SHALL BE MAINTAINED, IN ACCORDANCE WITH CHAPTER 33 OF THE INTERNATIONAL FIRE CODE; FIRE SAFETY DURING CONSTRUCTION

2. ALL WORK SUBJECT TO FIELD INSPECTION AND CORRECTION(S) AS IDENTIFIED AT THE TIME OF THE ON-SITE INSPECTION; ALL WORK SHALL BE COMPLIANT WITH THE APPLICABLE STANDARDS AND CODES; TO INCLUDE THE ADOPTED EDITION OF THE INTERNATIONAL FIRE CODE AND THE CITY'S MUNICIPAL CODE.

3. ALL FIRE ALARM AND FIRE SPRINKLERS SHALL BE SUBMITTED SEPARATELY AND DIRECTLY TO THE FIRE MARSHAL.

4. MODIFICATIONS FOR FUTURE TENANT IMPROVEMENT(S) MAY REQUIRE AN ALTERNATE PLANS RE-SUBMITTAL.

5. APPENDIX D FOR FIRE APPARATUS ACCESS ROAD/ALL ON-SITE PRIVATE UNDERGROUND FIRE SUPPRESSION WATER SUPPLY SHALL BE SUBMITTED TO THE FIRE MARSHAL (THIS INCLUDES PRIVATE HYDRANTS, UNDERGROUND FOR FDC'S AND FIRE SPRINKLER UNDERGROUND CONNECTIONS).

6. IFC APPENDIX D FIRE APPARATUS ACCESS ROADS, WHERE HYDRANTS ARE ON A FIRE APPARATUS ACCESS ROAD, THE MINIMUM WITH OF THE ROAD SHALL BE 26 FEET FOR A DISTANCE OF 20 FEET; 10 FEET IN EITHER DIRECTION.

7. IFC 503.3 MARKING WHERE REQUIRED BY THE FIRE CODE OFFICIAL, APPROVED SIGNS OR OTHER APPROVED NOTICES OR MARKINGS THAT INCLUDE THE WORDS NO PARKING FIRE LANE SHALL BE PROVIDED FOR FIRE APPARATUS ACCESS ROADS TO IDENTIFY SUCH ROADS OR PROHIBIT THE OBSTRUCTION THEREOF. THE MEANS BY WHICH FIRE LANES ARE DESIGNATED SHALL BE MAINTAINED IN A CLEAN AND LEGIBLE CONDITION AT ALL TIMES AND BE REPLACED OR REPAIRED WHEN NECESSARY TO PROVIDE ADEQUATE VISIBILITY.

8. IFC D103.6 FIRE APPARATUS ACCESS PARKING RESTRICTIONS--SIGNS: REQUIRED ROADWAYS MUST HAVE SIGNAGE FOR PARKING RESTRICTIONS AS FOLLOWS: SIGNS FOR NO-PARKING--FIRE LANE SHALL COMPLY WITH A MINIMUM DIMENSION OF 12 INCHES WIDE BY 18 INCHES HIGH AND HAVE RED LETTERS ON A WHITE REFLECTIVE BACKGROUND. SIGNS SHALL BE PROVIDED ON BOTH SIDES OF ALL STREETS THAT ARE LESS THAN 26 IN WIDTH IN ACCORDANCE WITH LOCAL STANDARDS FOR ACCESS AND FUTURE ENFORCEMENT; SIGNS FOR NO-PARKING MUST BE PROVIDED ON ONE SIDE OF ALL STREETS THAT ARE BETWEEN 26 AND 32 IN WIDTH ACCORDANCE WITH LOCAL STANDARDS FOR ACCESS AND FUTURE ENFORCEMENT.

9. IFC 506 WHERE REQUIRED ACCESS IS RESTRICTED WITH A GATE, AN APPROVED PADLOCK OR KEY SWITCH (FOR ELECTRONIC/AUTOMATED GATES) SHALL BE PROVIDED TO ALLOW FIRE DEPARTMENT ACCESS.

10. IFC 503.1.1 / D102 / D103 ROADWAYS TO ACCESS STRUCTURES: THE PERIMETER OF ALL STRUCTURES MUST BE WITHIN 150 FEET OF AN APPROVED ACCESS ROAD WITH A MINIMUM CLEAR WIDTH OF 20 FEET (26 FEET WHERE A HYDRANT IS LOCATED). BUILDING SHALL BE INSTALLED WITH AUTOMATIC FIRE SPRINKLERS AS AN ALTERNATIVE TO DISTANCE FROM A FIRE ACCESS ROAD.

11. IFC 507.5.4 FIRE PROTECTION WATER SUPPLY: UNOBSTRUCTED ACCESS TO FIRE HYDRANTS SHALL BE MAINTAINED AT ALL TIMES. THE FIRE DEPARTMENT SHALL NOT BE DETERRED OR HINDERED FROM GAINING IMMEDIATE ACCESS TO FIRE PROTECTION EQUIPMENT OR FIRE HYDRANTS REQUIRED ACCESS ROADWAYS AND HYDRANTS SHALL BE SERVICEABLE AND UNOBSTRUCTED PRIOR TO ACCESS CONSTRUCTION.

## SHEET INDEX

G002 CIVIL NOTES  
C050 EXISTING CONDITIONS & DEMOLITION PLAN  
C100 SITE PLAN  
C130 GRADING PLAN  
C600 CONSTRUCTION DETAILS

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DULUTH, MN 55802

CHECKED BY:  
JD

DRAWN BY:  
NCK

PROJECT NO.  
22163.3

RELEASE DATE:  
5/4/26

REVISIONS:  
REV. DATE DESCRIPTION

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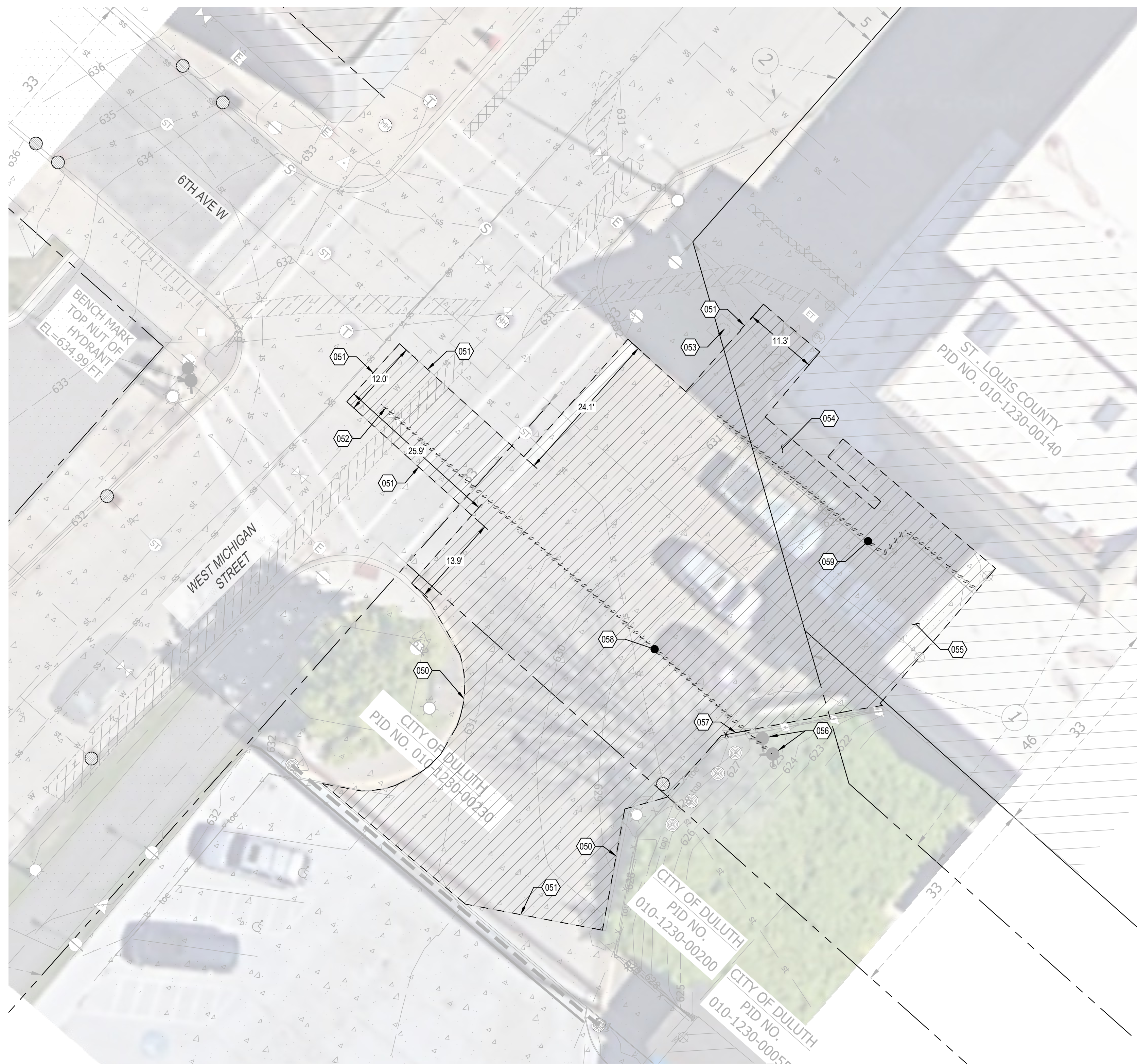
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**G002**  
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LEGEND					
	CONCRETE SURFACE		TOP OF BANK OR GRADE BREAK		HYDRANT
	BITUMINOUS SURFACE		CONCRETE CURB & GUTTER		WATER VALVE
	EXISTING BUILDINGS		RETAINING WALL		UTILITY VAULT
	FIBER OPTIC DUCT AS LOCATED		FENCE LINE		TELEPHONE MANHOLE
	ELECTRIC DUCT AS LOCATED		HAND RAILING LINE		LIGHT POLE
			UNDERGROUND ELEC		SIGN
			UNDERGROUND FIBER OPTIC		DECIDUOUS TREE
			UNDERGROUND GAS		POST/BOLLARD
			SANITARY SEWER		SPOT ELEVATION
			STORM SEWER		ROOF DRAIN
			UNDERGROUND WATER LINE		HANDI CAPPED PARKING SPOT
			CENTER LINE		# REFER TO SURVEYOR'S NOTES
			RIGHT OF WAY LINE		CONTROL POINT
			BOUNDARY LINE AS SURVEYED		FOUND MAG NAIL
			EXISTING PLAT LINE		ELEC PEDESTAL
			ADJACENT PARCEL LINE		ELEC MANHOLE
	MISC MANHOLE		SANITARY MANHOLE		ELEC TRANSFORMER
	SANITARY MANHOLE		STORM MANHOLE		
	STORM MANHOLE		CATCH BASIN		

050 DEMOLITION	
050	PROTECT EXISTING CURB TO REMAIN.
051	DEMO CONCRETE ALONG JOINT LINES. CONTRACTOR TO DEMO AND REPLACE FULL CONCRETE PANELS.
052	CAP EXISTING HYDRANT SERVICE LINE
053	PROTECT EXISTING TREE TO REMAIN.
054	DEMO SIDEWALK ON TOP OF RETAINING WALL. PROTECT BUILDING FOUNDATION DURING DEMOLITION.
055	CONTRACTOR TO DEMO AND REPLACE FULL CONCRETE PANELS.
056	EXISTING HYDRANT AND POST VALVE INDICATOR TO BE REMOVED.
057	FENCE (APPROX 45') TO BE DEMOED AS NECESSARY TO ACCOMMODATE IMPROVEMENTS AND CONDUIT ROUTING.
058	EXISTING FIRE HYDRANT SERVICE LATERAL TO BE REMOVED.
059	DEMO EXISTING RAILING

**1 EXISTING CONDITIONS & DEMOLITION PLAN**  
 SCALE: 1"=10'



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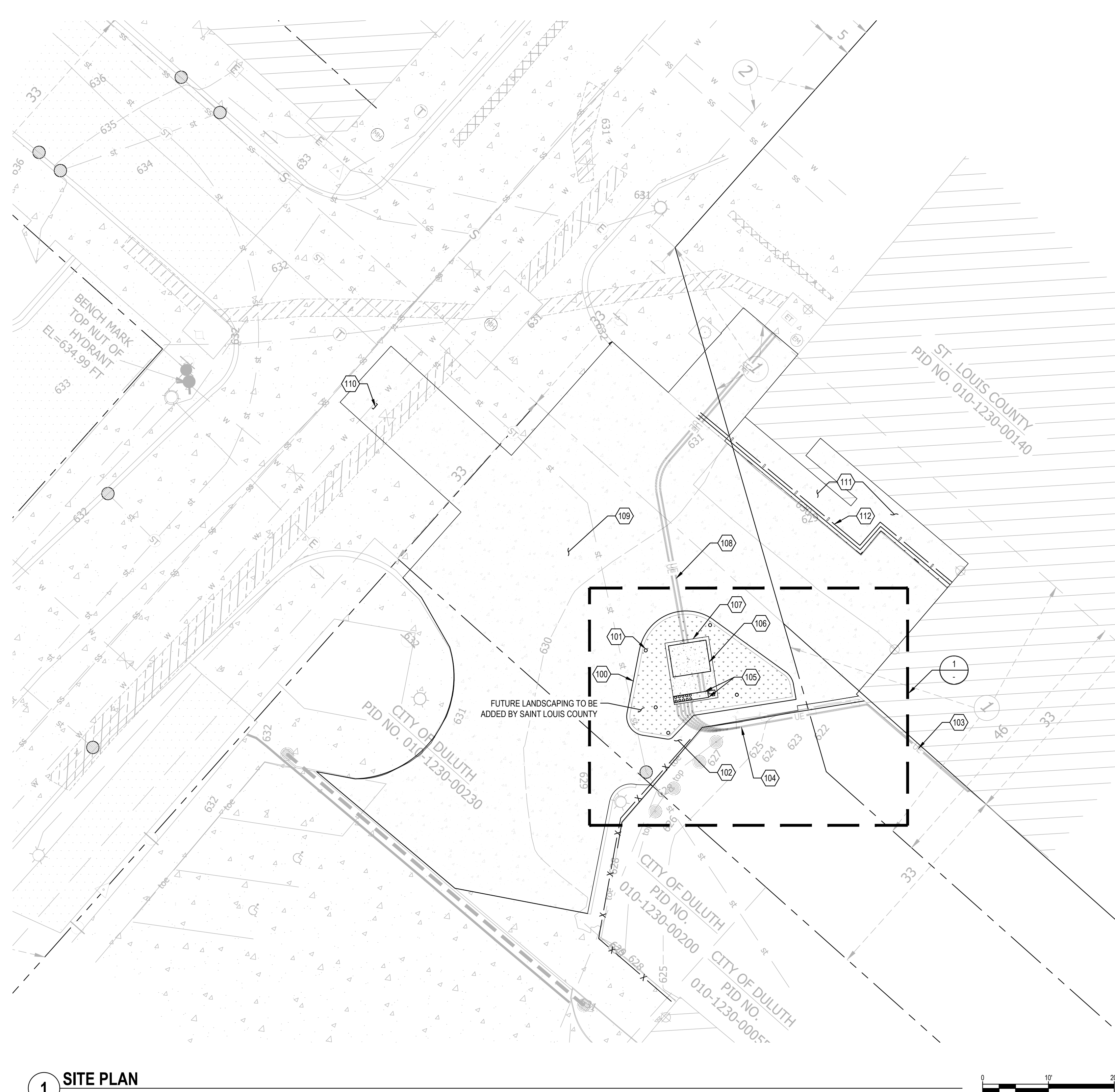
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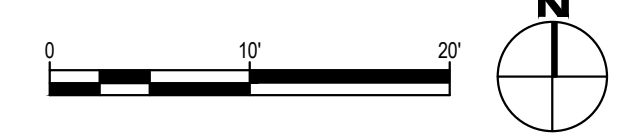
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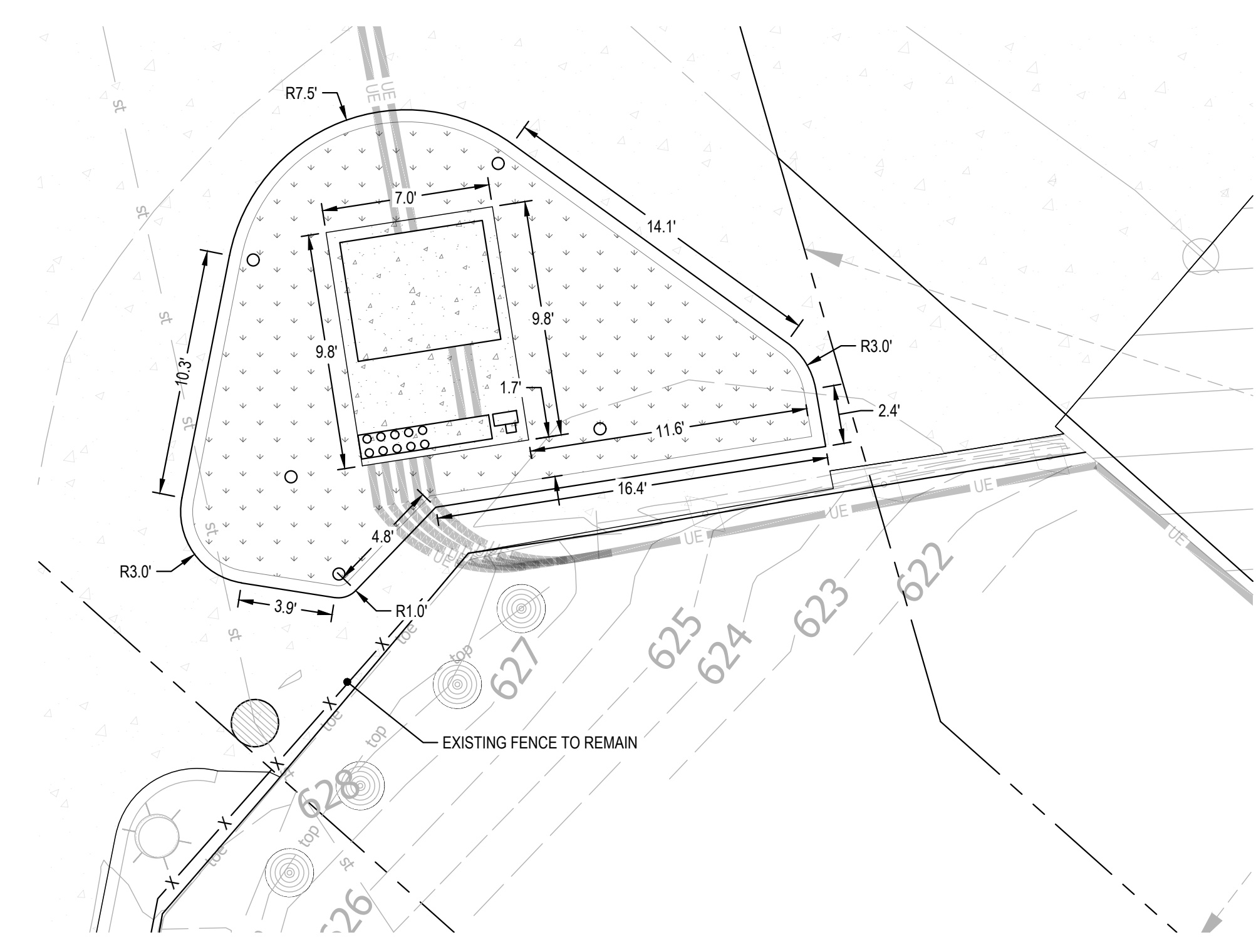


**1 SITE PLAN**  
 SCALE: 1"=10'

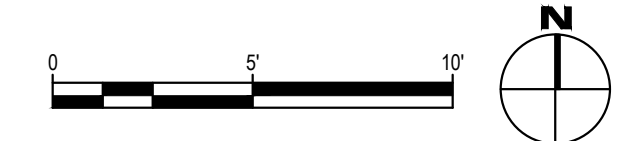


**KEYNOTES:**

100 SITE PLAN		
100	PROPOSED 6" CURB	
101	PROPOSED BOLLARDS (TYP)	4 C600
102	CONCRETE DRAINAGE PATH BETWEEN CONCRETE AND EX RETAINING WALL. SLOPE TO EXISTING CATCH BASIN. SEE GRADING PLAN.	
103	PROPOSED SECONDARY CONDUITS RUN ALONG BUILDING FACE ABOVE GRADE. SEE ELECTRICAL PLANS FOR DETAILS.	
104	PROPOSED (10) SECONDARY CONDUITS WRAP AROUND EXTERIOR WALL AND MOUNTED ON WALL TO ROUTE FROM CABINET TO FACE OF BUILDING. MIN 2" COVER OVER CONDUITS.	
105	PROPOSED TRANSITION CABINET AND UTILITY METER. SEE ELECTRICAL PLANS FOR DETAIL. INSTALLATION BY CONTRACTOR.	2 C600
106	PROPOSED TRANSFORMER. INSTALLATION BY MNPOWER. CONCRETE PAD INSTALLATION BY CONTRACTOR.	3 C600
107	PROPOSED CONCRETE PAD FOR PROPOSED TRANSFORMER. CONCRETE PAD INSTALLATION BY CONTRACTOR.	
108	PROPOSED CONDUIT ROUTING FROM EXISTING TRANSFORMER TO PROPOSED TRANSFORMER BY CONTRACTOR.	1 C600
109	PROPOSED CONCRETE	
110	CAP EX HYDRANT SERVICE LINE	
111	PROPOSED SIDEWALK TO BE CONSTRUCTED ON TOP OF RETAINING WALL	
112	PROPOSED HANDRAIL. MIN. 36" HIGH WITH POSTS ANCHORED TO PROPOSED CONCRETE SIDEWALK. HANDRAIL TO BE DESIGN BUILD BY CONTRACTOR WITH OWNER REVIEW AND APPROVAL.	



**1 CONCRETE PAD & CURB**  
 SCALE: 1"=5'



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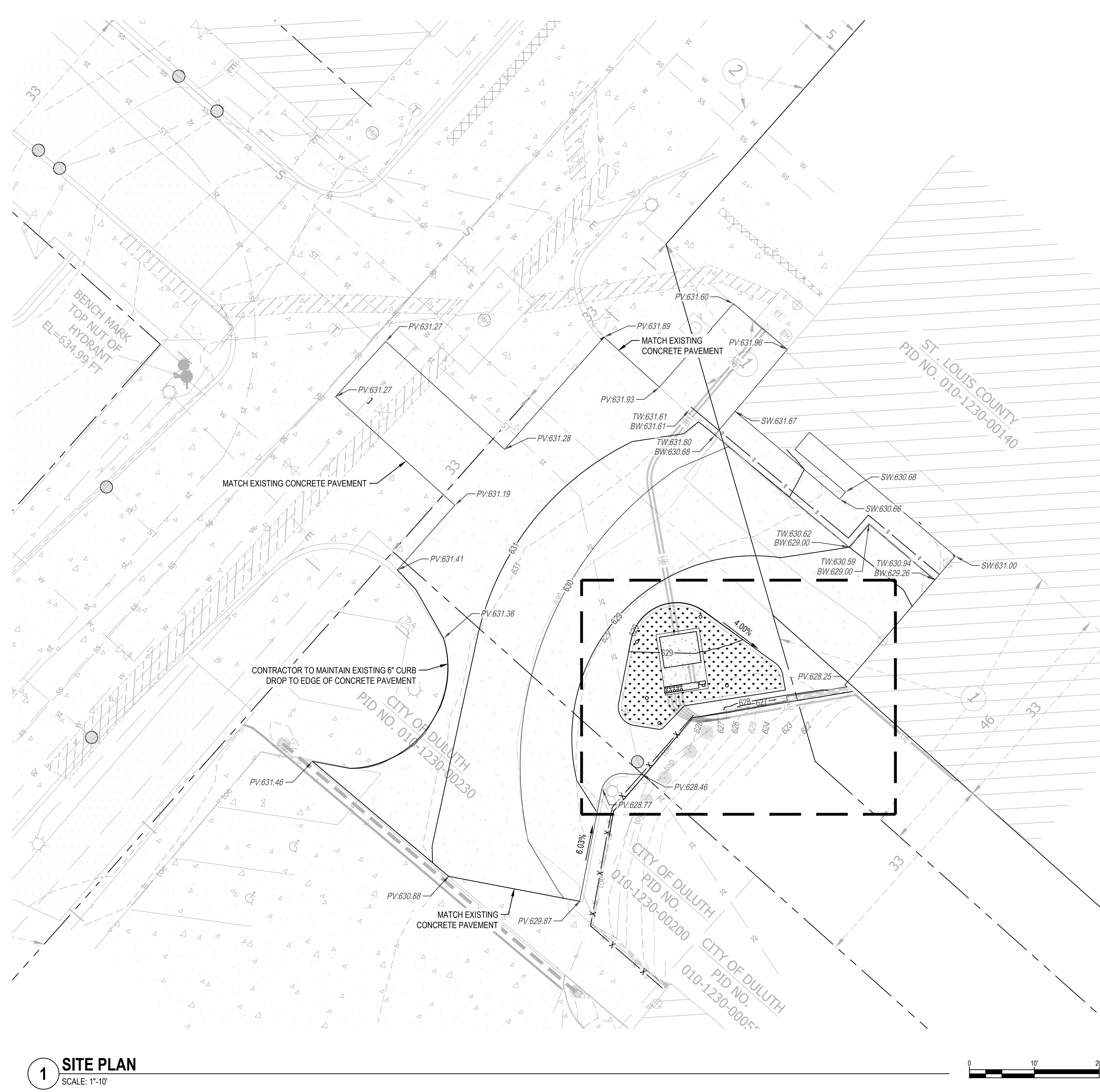
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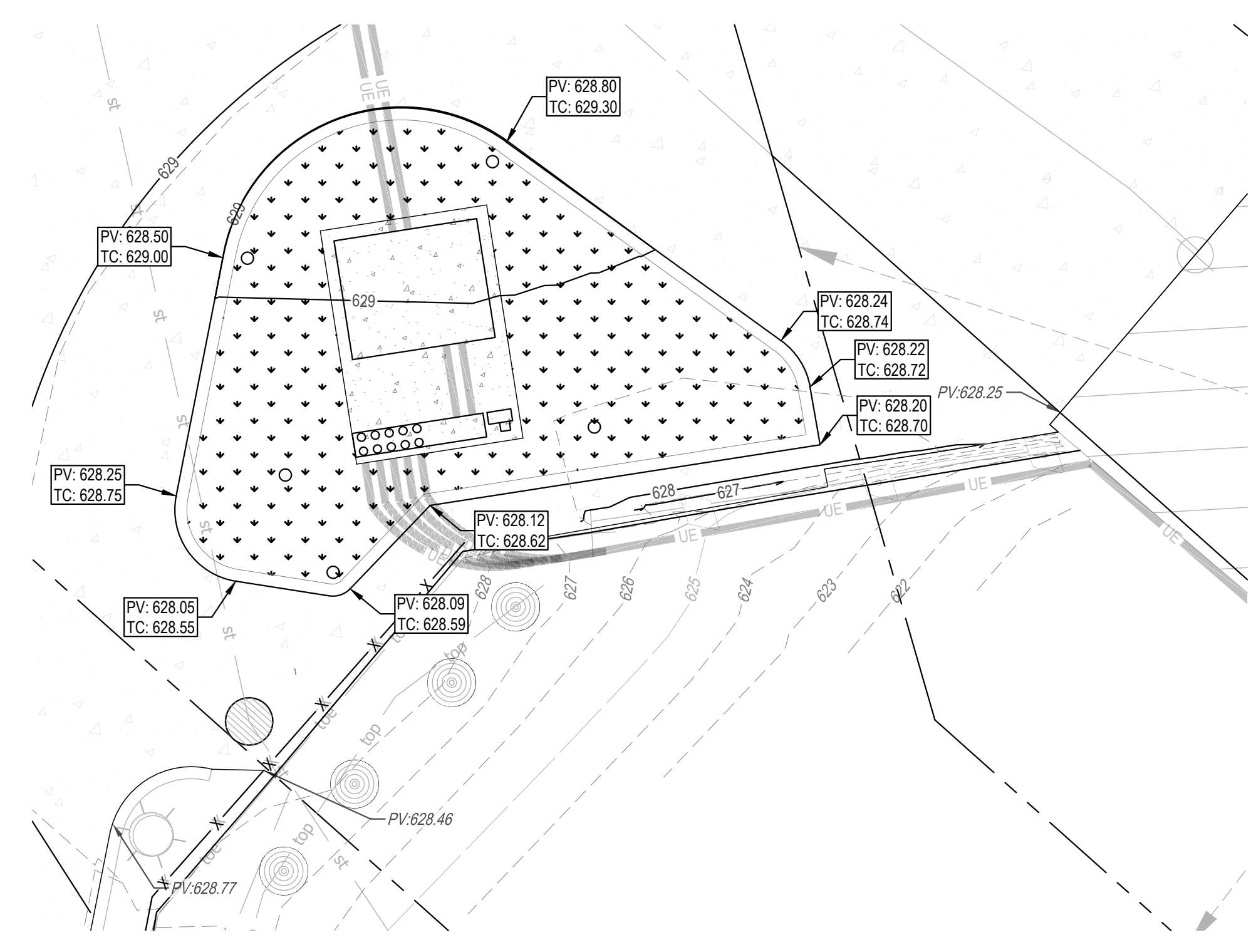
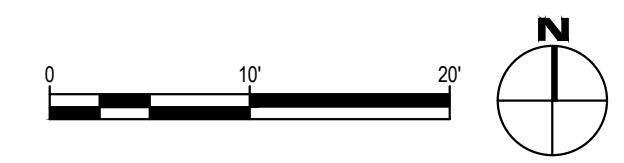
REV.	DATE	DESCRIPTION

SET NO.

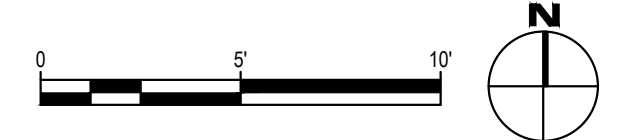
SHEET NO.  
**C100**  
 03 OF 05



**1 SITE PLAN**  
SCALE: 1"=10'



**2 CONCRETE PAD & CURB**  
SCALE: 1"=5'



**CONTRACTOR NOTE:**  
ALL EXISTING UTILITIES SHOWN ON PLANS ARE TO BE VERIFIED HORIZONTALLY AND VERTICALLY PRIOR TO ANY CONSTRUCTION. ALL EXISTING FEATURES INCLUDING BURIED UTILITIES ARE SHOWN AS INDICATED ON RECORD MAPS AND SURVEYS FURNISHED BY OTHERS WINDSOR ASSUMES NO LIABILITY FOR THE ACCURACY OF THOSE RECORDS AND SURVEYS. CONTACT THE UTILITY OWNER/AGENCY FOR THE FINAL LOCATION OF EXISTING UTILITIES IN AREAS CRITICAL TO CONSTRUCTION.

**GENERAL GRADING NOTE:**  
1. TOP OF WALL AND BOTTOM OF WALL SPOT ELEVATION REFER TO THE FINAL GRADE AT THE TOP AND BOTTOM OF WALL AND DO NOT INDICATE FOOTING DEPTH ETC. RETAINING WALL IS TO REMAIN WITHOUT IMPROVEMENTS OR MODIFICATIONS.

**ARCHITECTURE  
ADVANTAGE**

9715 Piedmont Avenue - Duluth, MN 55811  
Electrical Engineering  
Suite 420 375 Jackson Street - Saint Paul, MN 55101  
Phone: 651.224.4831 - Email: info@architectureadvantage.com

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SHEET TITLE:  
**GRADING PLAN**

This sheet may be a reduced size copy. The drawing scales apply to full scale sheets.

**SLC DEPOT**  
506 WEST MICHIGAN ST.  
DULUTH, MN 55802

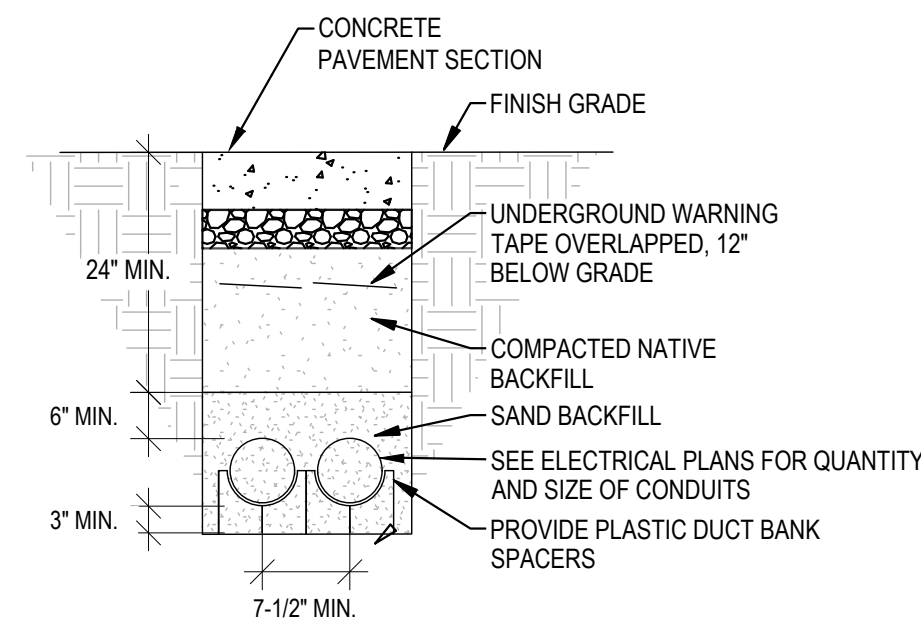
PROJECT NO.: 22163.3  
DRAWN BY: NCK  
CHECKED BY: JD

RELEASE DATE: 5/4/26

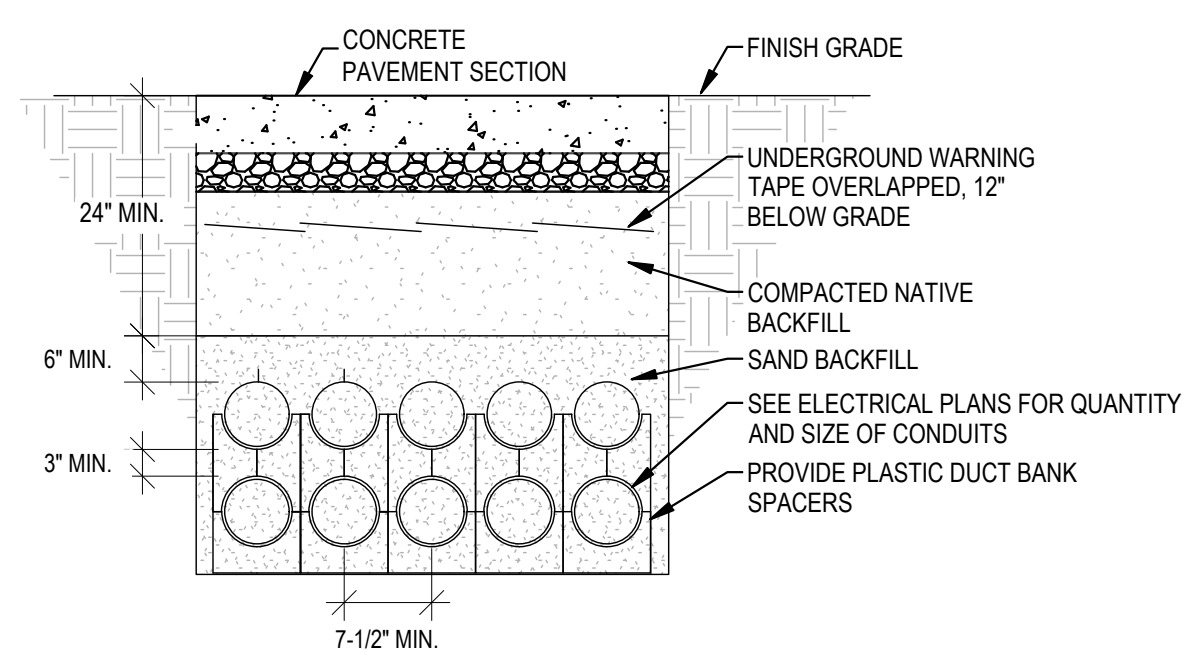
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REV. DATE DESCRIPTION

SET NO.

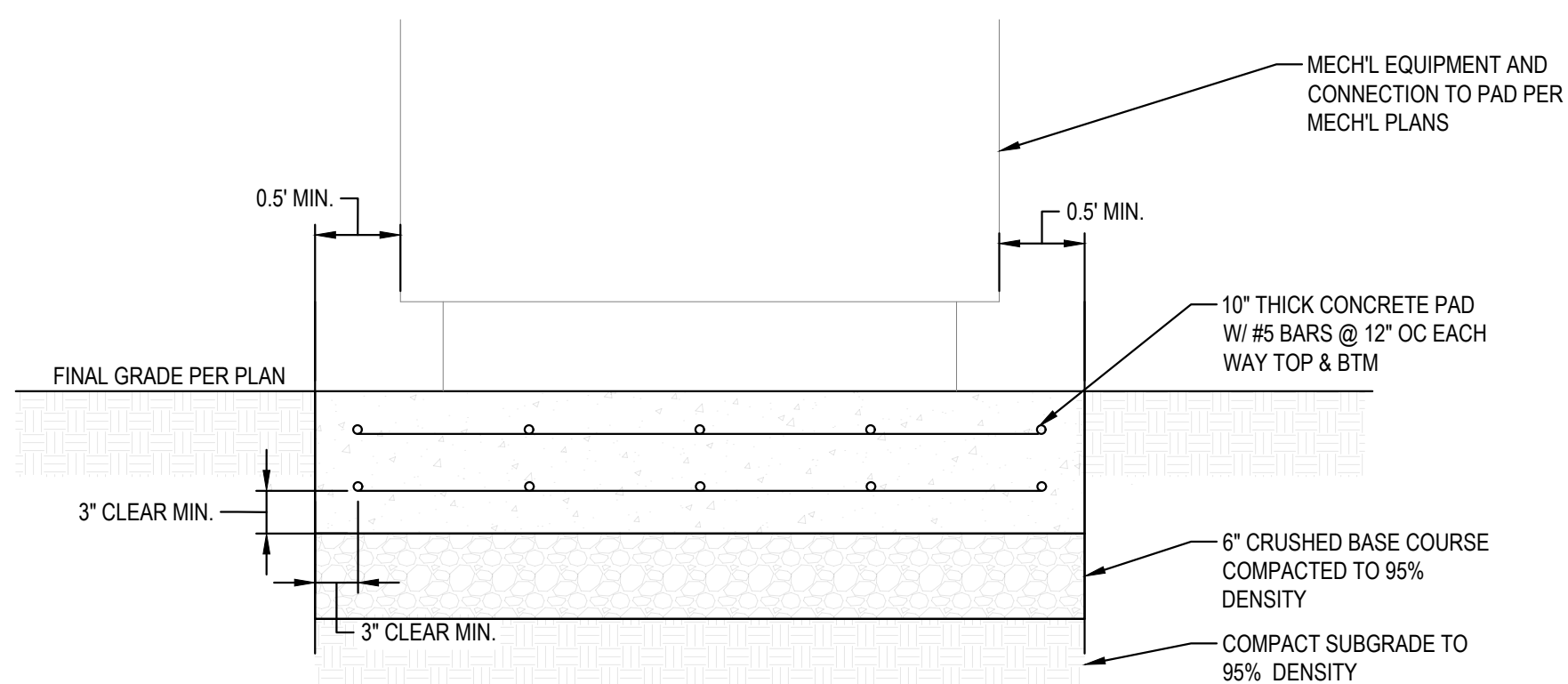
SHEET NO.  
**C130**  
04 OF 05



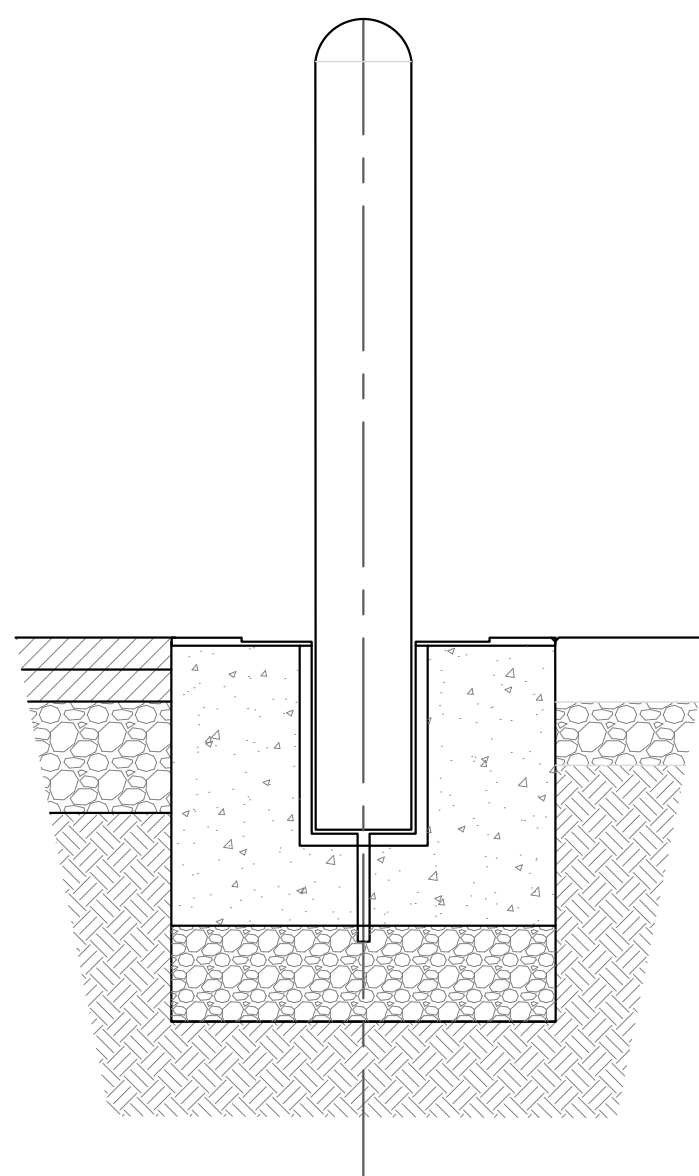
**1 CONDUIT ROUTING DETAIL**  
SCALE: NTS



**2 CONDUIT ROUTING DETAIL**  
SCALE: NTS



**3 TRANSFORMER PAD CONCRETE DETAIL**  
NOT TO SCALE



**4 REMOVABLE BOLLARD**  
NOT TO SCALE

**PAVEMENT REPLACEMENT (TYPE CX)**

DESCRIPTION: REMOVE CONCRETE, RESTORE AND COMPACT BASE, PLACE DOWELS AND REINFORCEMENT BARS, FURNISH AND PLACE CONCRETE, SAW AND SEAL JOINTS.

**Notes**

- Place saw cut at least 2' up stream or 5' down stream from any transverse joints in the adjacent lane.
- Drill concrete pavement utilizing a drill bit diameter of > 1/8" inch larger than dowel/rebar bar diameter.
- Dowel bar basket required when longitudinal length of the repair is > 15'. Place dowel bar assemblies at 15' centers, when repair length exceeds 30'.

**WORK TO BE DONE**

- See Full Depth Repair (Type CD) for additional information.
- Saw full depth & remove in place concrete pavement. Restore and compact in place base.
- Drill in place concrete for dowel bars. Eliminate inside 5 dowels for (Type CD-LV) repairs.
- Furnish and install dowels. Secure dowels to the in place concrete with an approved non shrink grout or epoxy adhesive. Coat free end of dowels with a form coating material meeting Spec. 3982.
- If matching in place transverse joints, drill and grout epoxy coated tie bars into the adjacent lane. ~~Drill and grout in place concrete bars not require if repair length is under 75'.~~
- OR
- Isolate all transverse pavement joints and cracks. Furnish and install joint filler 3782 between the adjacent in place lane and the (Type CX) repair. (Incidental)
- As needed, furnish (1.0" Diam.) Dowel Bar Assemblies, in accordance with Standard Plate 1183.
- Clean vertical surfaces of in place concrete.
- Place epoxy coated supplemental reinforcing bars over culverts as needed. See Standard Plate 1878.
- Furnish and place Concrete Mix Number 3852.
- Furnish and install epoxy coated rebar in conc. repair, located 3" from end of dowel bar.
- Vibrate, finish to grade and slope, edge, texture, and apply cure.
- Green saw joints over dowel bar baskets.
- Saw and seal joints in accordance with Joint Repair (Type A1) detail. (Incidental)

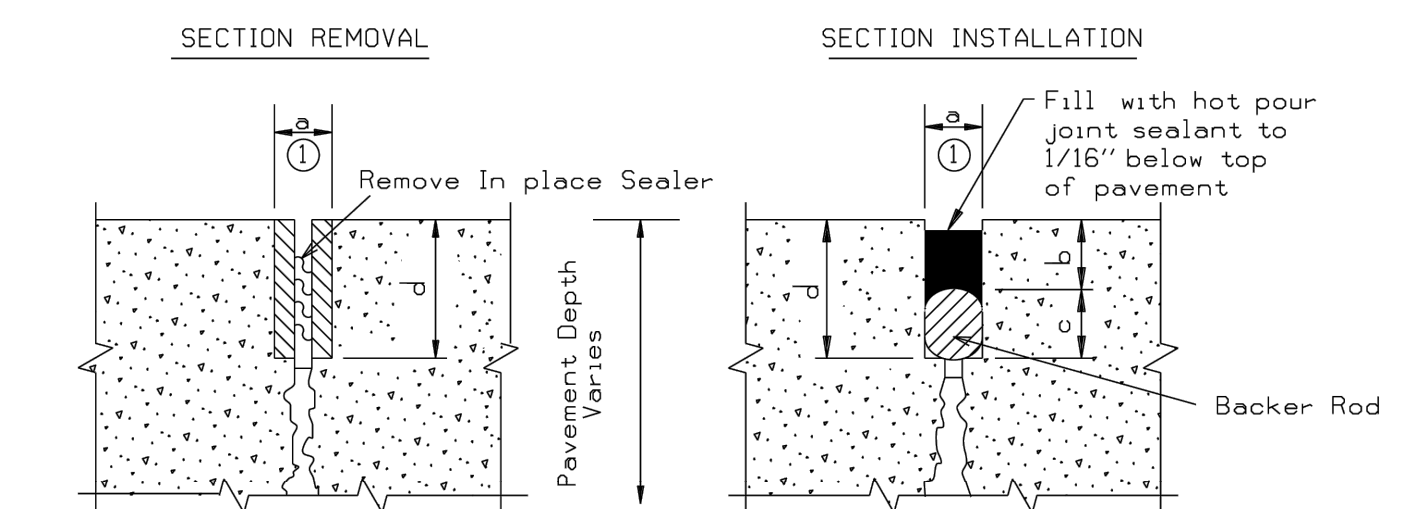
**BASIS OF PAYMENT**

- 2302 Full Depth Repair (Type CD) (Lin. Ft.)
- 2302 Drill and Grout Reinforcement Bars (Epoxy Coated) (Each)
- 2302 Dowel Bar (Each)
- 2302 Supplemental Reinforcement Bars (Epoxy Coated) (Pound)
- 2302 Pavement Replacement (Type CX) (Sq. Yd.)

S.P. NO. DATE: Modified January XX, 2026 SHEET OF SHEETS

**5 PAVEMENT REPLACEMENT (TYPE CX)**  
SCALE: NTS

VARIABLE WIDTH JOINT CRACK REPAIR / JOINT REPAIR (TYPE A1)  
DESCRIPTION: IN NEWLY CONSTRUCTED REPAIRS OR IN PLACE JOINTS OR CRACKS. SAW, CLEAN, SEAL TRANSVERSE AND / OR LONGITUDINAL PAVEMENT JOINTS OR CRACKS.



AREA TO BE REMOVED BY SAWING, IN PLACE JOINT OR CRACK MAY CONTAIN JOINT SEALER

**NOTES**

- On roadways with posted speed limits of 45 mph or greater the maximum recommended 'a' dimension is 3/8 inch.

JOINT WIDTH (INCHES)	SEALANT BEAD THICKNESS (INCHES)	BACKER ROD DIAMETER (INCHES)	MINIMUM JOINT DEPTH (INCHES)	ESTIMATED QUANTITY (ft <sup>3</sup> /gal)	ESTIMATED QUANTITY (lb/lf)
a	b	c	d		
1/4	1/4	3/8	11/16	268	0.035
3/8	3/8	1/2	15/16	119	0.079
1/2	1/2	5/8	1-3/16	67	0.140
5/8	5/8	3/4	1-7/16	43	0.219
3/4	3/4	7/8	1-11/16	30	0.316
7/8	7/8	1.0	1-15/16	22	0.430
1.0	1.0	1-1/8	2-3/16	17	0.562
1-1/8	1-1/8	1-1/4	2-7/16	13	0.711
1-1/4	1-1/4	1-3/8	2-11/16	11	0.877

**WORK TO BE DONE**

- Remove in place joint sealer, if applicable.
- Saw both joint faces to configuration shown then immediately water flush the joint or crack.
- Clean and dry joint or crack by sandblasting and air blasting.
- Furnish and install backer rod of appropriate diameter in joint or crack opening. Furnish and install backer rod when joints are 1/4" wide or greater.
- Fill joint or crack with Joint and Crack Sealer (Specification 3725 Hot Poured).
- To prevent tracking of the Joint and Crack Sealer use tissue paper if necessary.

**BASIS OF PAYMENT**

- 2302 Joint Repair (Type A1) (Lin. Ft.)

S.P. NO. DATE: APRIL 22, 2016 SHEET OF SHEETS

**6 JOINT REPAIR (TYPE A1) DETAIL**  
SCALE: NTS

