



Staff Report for PLIUP-2604-0002

Application Information:

Application Type: Interim Use Permit for Overflow Parking

Location of Subject: 1909 - 1921 London Road

Legal Description: 010-1460-02960, 010-1460-02950
010-1460-02940, 010-1460-02930

Applicant: Venture Pass Partners LLC

Applicant Contact: n/a

Agent: n/a

Agent Contact: n/a

Staff Contact: Chris Lee, clee@duluthmn.gov

Deadline for Action:

Application Date: April 2, 2026

Date Extension Letter Mailed: April 12, 2026

60 Days: June 1, 2026

120 Days: July 31, 2026

Site Visit Date: April 24, 2026

Sign Notice Date: April 28, 2026

Neighbor Letter Date: April 22, 2026

Number of Letters Sent: 66

Proposal:

Applicant is seeking an interim use permit for overflow parking for a proposed restaurant.

Recommended Action:

Staff recommends approval.

Zoning and Land Use:

	Current Zoning	Existing Land Use	Future Land Use Map Designation
Subject	F-2	Residential	Neighborhood Mixed Use
North	R-2	Residential	Urban Residential
South	F-2	Dental Office	Neighborhood Mixed Use
East	F-2	Restaurant	Neighborhood Mixed Use
West	F-2	Residential	Neighborhood Mixed Use

Summary of Code Requirements:

50-20.4.O. Overflow Parking Area (paraphrased): 1. Interim use permits for Overflow Parking Areas may be granted for up to two years. If the applicant has documented an ongoing parking need, with a parking study documenting the location, use, and adequacy of existing parking in the area, the interim use permit may be approved for up to 5 years; 2. If located in an area with a need for additional parking, the Overflow Parking Area shall be public or shared parking. 3. Overflow Parking Areas may propose alternatives to paving and landscaping requirements; however, if paving is provided, all requirements of Sections 50-24 and 50-25 must be met, if applicable; 4. Applicant must work with City Engineering on a stormwater and erosion plan 5. Prior to establishment of the parking area, applicant shall provide financial security, in the amount necessary to reestablish the area to natural vegetation, once the overflow parking area is terminated. 6. The interim use permit shall terminate upon change in ownership of the property and is not transferable.

UDC Section 50-37.10. Interim Use Permits. The Planning Commission shall approve the application or approve it with modifications if the commission determines that the application meets the following criteria: 1. The application is consistent with the Comprehensive Land Use Plan; 2. The application complies with all applicable provisions of this Chapter, including without limitation any use-specific standards applicable to the proposed use, development or redevelopment, and is consistent with any approved district plan for the area; 3. Without limiting the previous criteria, the commission may deny any application that would result in a random pattern of development with little contiguity to existing or programmed development or would cause anticipated negative fiscal or environmental impacts on the community.

Comprehensive Plan Governing Principle and/or Policies and Current History (if applicable):

Principle #1 – Reuse previously developed lands – The overflow parking will accommodate a new restaurant to be built along an existing commercial corridor.

Principle #6 – Support economic growth sectors – The request for additional parking will allow for a new restaurant and increase commercial tax base.

Future Land Use: Neighborhood Mixed Use – A transitional use between more intensive commercial uses and purely residential neighborhoods. Includes conversions of houses to office or live-work spaces. May include limited commercial-only space oriented to neighborhood or specialty retail markets.

History: The properties contain 3 homes that will be demolished.

Review and Discussion Items:

Staff finds:

1. The applicant is seeking an interim use permit for 11 overflow parking stalls for a new restaurant. The site is proposed to contain 35 total parking stalls which is over the maximum of 24. The overflow parking will be integrated into the overall parking lot with stormwater and landscaping.
2. A restaurant is a permitted use in the F-2 zone district. The applicant has indicated that this franchise of restaurant at this location will generate a level of traffic that requires on average 13 employees to have parking as well as the customers.
3. UDC Sec. 50-20.4.O. (Use Specific Standards – Overflow Parking Areas). The applicant has indicated that this will be a paved parking that is landscaped to meet the standards of 50-24. The applicant shall submit escrow to cover the cost of removal of this parking and restoration to a natural surface if the interim use permit expires.
4. UDC Sec. 50-24 (Parking and loading). Parking maximums for a restaurant are 10 stalls per 1,000 square feet of gross floor area. The proposed restaurant is 2,379 square feet which has a required maximum parking limit of 24 stalls.
5. UDC Sec. 50-25 (Landscaping). The applicant has submitted plans that indicates 5 trees will be planted in the parking area meeting the canopy coverage requirement. Other landscaping is shown to meet landscaping standards for development.
6. No public, agency, or City comments have been received at the drafting of this report.

Staff Recommendation:

Based on the above findings, Staff recommends that Planning Commission approve the interim use permit with the following conditions:

1. The project must be constructed and limited to the plans submitted with the application.
2. The permit is valid for 2 years from the date of approval.
3. The applicant will submit financial security to restore the site should the permit be extinguished.
4. The permit will lapse if no activity takes place within 1 year of approval.
5. Any alterations to the approved plans that do not alter major elements of the plan may be approved by the Land Use Supervisor without further Planning Commission approval; however, no such administrative approval shall constitute a variance from the provisions of Chapter 50.



Staff Report Map:

Figure 1.

Site Plan:

Figure 2.

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Certificate of Survey:

Figure 3

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Map for PLIUP-2604-0002:

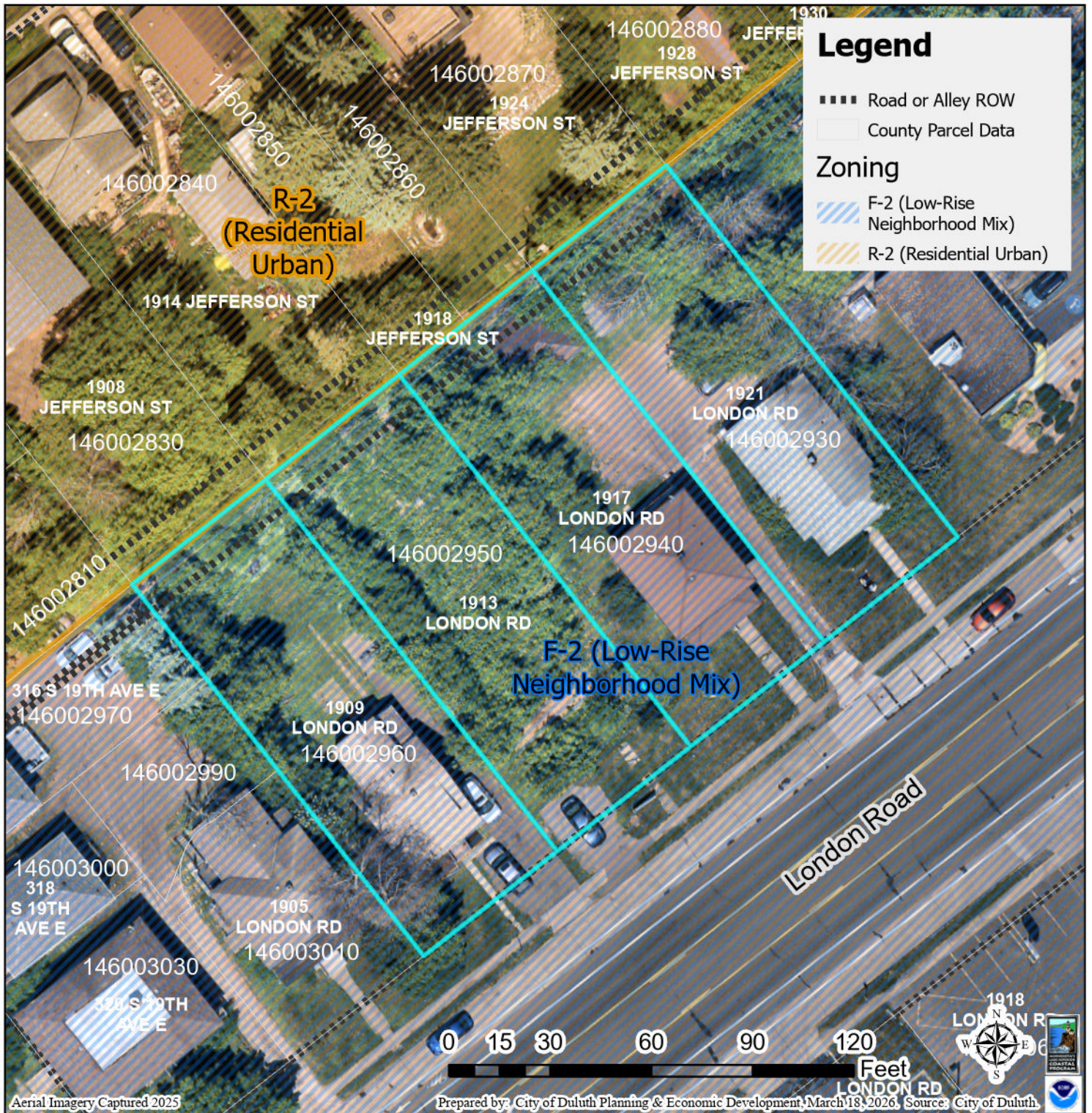
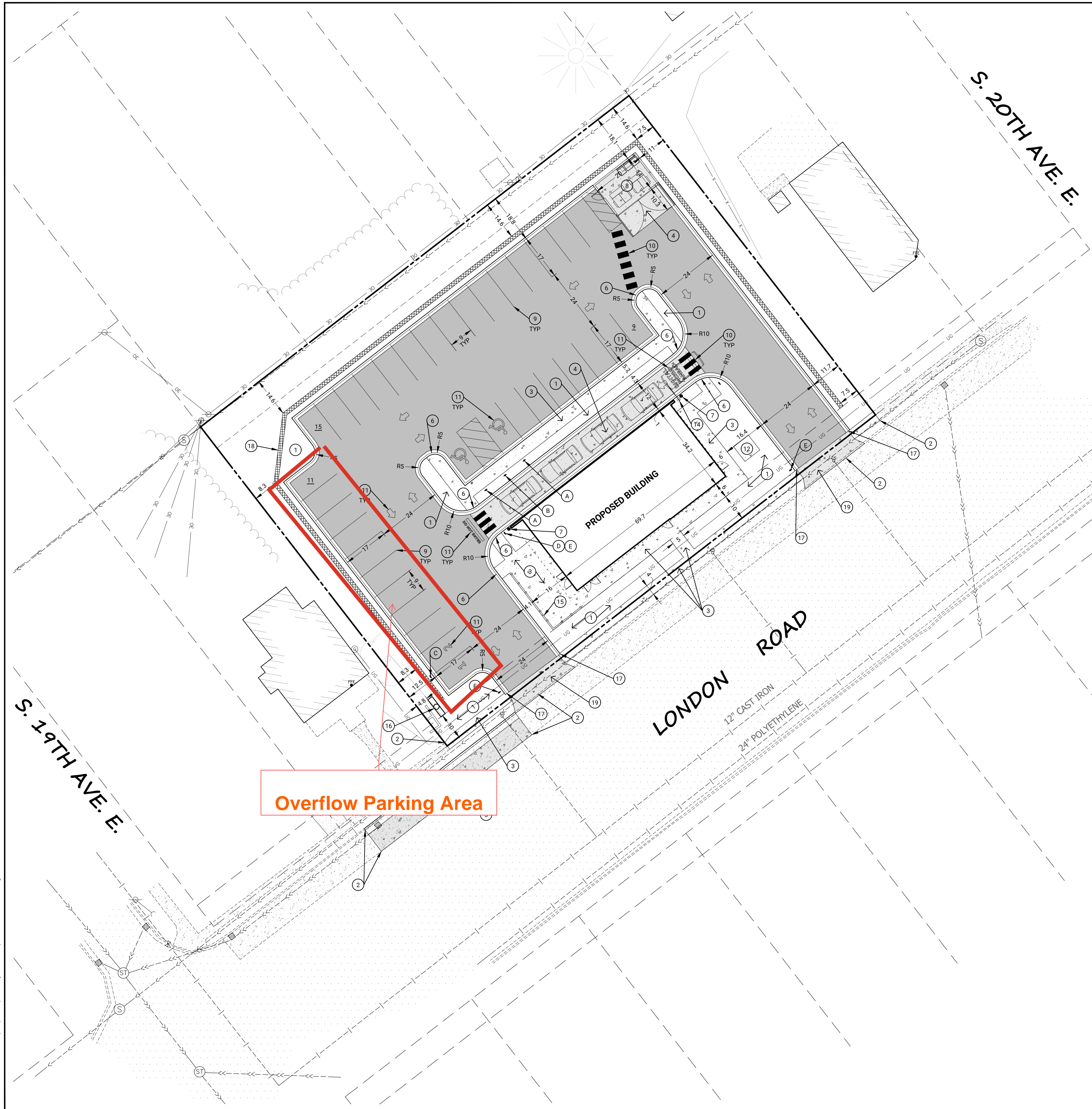


Figure 1.

Aerial imagery captured in 2025, showing the property at 1909, 1913, 1917, and 1921 London Road highlighted.

The City of Duluth has tried to ensure that the information contained in this map or electronic document is accurate. The City of Duluth makes no warranty or guarantee concerning the accuracy or reliability. This drawing/data is neither a legally recorded map nor a survey and is not intended to be used as one. The drawing/data is a compilation of records, information and data located in various City, County and State offices and other sources affecting the area shown and is to be used for reference purposes only. The City of Duluth shall not be liable for errors contained within this data provided or for any damages in connection with the use of this information contained within.



SIGNAGE AND PAVEMENT MARKING NOTES

- ALL SIGNS SHALL BE PLACED 18" MINIMUM BEHIND BACK OF CURB UNLESS OTHERWISE NOTED.
- SIGNS SHALL INCLUDE HARDWARE, POST, FOOTING, CASING, AND ALL APPURTENANCES REQUIRED FOR TYPICAL INSTALLATION.
- PARKING LOT STRIPING AND PAVEMENT MARKINGS SHALL BE 4" SOLID WHITE PAINT. ALL PAVEMENT LETTERING SHALL BE 12" HEIGHT.
- ALL SIGNS AND PAVEMENT MARKINGS SHALL BE PER THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD).

SIGN SCHEDULE

- A. HANDICAP PARKING SIGN: R7-8m W/R7-8b (12' X 18')
- B. NO PARKING SIGN: R8-3a (12' X 18')
- C. PULL AHEAD PARKING SIGN: BY TENANT
- D. DO NOT ENTER SIGN: R5-1 (24' X 24')
- E. STOP SIGN: R1-1 (24' X 24')

SITE DATA

EXISTING ZONING:	F-2, LOW-RISE NEIGHBORHOOD MIX
PROPOSED ZONING:	F-2, LOW-RISE NEIGHBORHOOD MIX
PARCEL AREA:	30,202 SF
IMPERVIOUS AREA:	
BUILDING:	2,379 SF 8%
PAVEMENTS:	18,552 SF 61%
TOTAL:	20,931 SF 69%
PERVIOUS AREA:	9,271 SF 31%

PARKING SUMMARY

REQUIREMENT	
10 SPACES PER 1,000 GSF MAX:	24
PROVIDED	
STANDARD STALLS:	33
HANDICAP STALLS:	2
TOTAL STALLS:	35
NOTE:	INTERIM USE PERMIT REQUIRED FOR ADDITIONAL PARKING OVER MAXIMUM ALLOWED

LEGEND

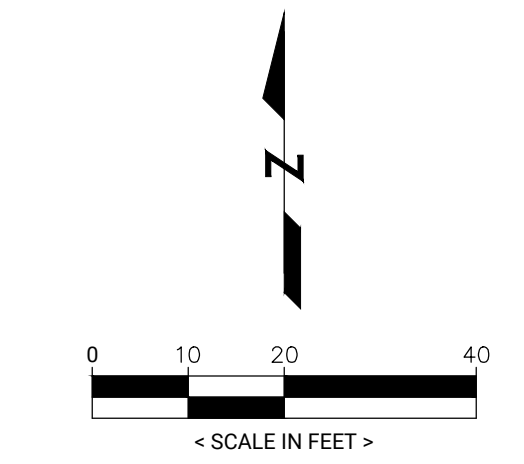
EXISTING	PROPOSED	
		PROPERTY LINE
		EASEMENT LINE
		BITUMINOUS PAVEMENT
		CONCRETE WALK/SWOOP
		CONCRETE PAVEMENT
		CURB LINE
		STORM SEWER
		SANITARY SEWER
		WATER MAIN
		OVERHEAD ELECTRIC
		UNDERGROUND GAS
		UNDERGROUND FIBER
		SANITARY MANHOLE
		STORM CATCH BASIN
		STORM MANHOLE
		HYDRANT
		GATE VALVE
		LIGHT POLE
		UTILITY POLE
		CURB STOP
		GAS METER
		OTHER SYMBOLS

SITE PLAN NOTES

- ALL DIMENSIONS ARE TO FACE OF CURB AND OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - ALL CURB AND GUTTER SHALL BE B612 UNLESS OTHERWISE NOTED.
 - TYPICAL PARKING STALLS ARE 9' X 17' UNLESS OTHERWISE NOTED.
 - ALL PEDESTRIAN RAMPS SHALL MEET CURRENT ADA STANDARDS.
 - UNLESS OTHERWISE SHOWN IN THE PLANS, CONTRACTOR SHALL PROVIDE CONTROL JOINTS AND EXPANSION JOINTS IN SLAB ON GRADE, SIDEWALKS, AND DRIVES PER THE FOLLOWING REQUIREMENTS:
 - CONTROL JOINTS: WALKS @ 8' SPACING
OTHERS @ 10' SPACING
 - EXPANSION JOINTS: WALKS @ 24' SPACING
OTHERS @ 40' SPACING
- SAWCUT CONTROL JOINTS MINIMUM 1/4 CONCRETE THICKNESS.
- PROVIDE EXPANSION JOINT AT ALL POINTS WHERE A CHANGE IN PAVEMENT THICKNESS OCCURS AND WHERE NEW PAVEMENT WILL MATCH EXISTING PAVEMENT.
- DOWEL ALL EXPANSION JOINTS AT 24" SPACING MAX.

KEYNOTES

- * SEE ARCHITECTURAL SITE PLANS FOR ADDITIONAL SITE PLAN NOTES.
- 1. LANDSCAPE AREA. SEE LANDSCAPE PLAN.
- 2. MATCH EXISTING CURB/SIDEWALK/PAVEMENT.
- 3. CONCRETE WALK/PATIO.
- 4. CONCRETE PAVEMENT/APRON.
- 5. REPLACE PAVEMENT IN KIND.
- 6. PEDESTRIAN CURB RAMP.
- 7. 6" CONCRETE FILLED PIPE BOLLARD.
- 8. TRASH ENCLOSURE.
- 9. 4" SOLID WHITE LINE.
- 10. CROSS-WALK PAVEMENT MARKINGS, SOLID WHITE.
- 11. SOLID WHITE PAVEMENT MARKING.
- 12. TRANSFORM PAD. FINAL LOCATION TO BE CONFIRMED BY CONTRACTOR AND ELECTRIC PROVIDER.
- 13. LIGHT POLE AND BASE, TBD.
- 14. CLEARANCE/HEIGHT RESTRICTION BAR PER TENANT.
- 15. PATIO FENCE, SEE ARCHITECTURAL PLANS.
- 16. PYLON SIGN.
- 17. TAPER OUT CURB.
- 18. CAST-IN-PLACE CONCRETE RETAINING WALL WITH CAST IRON FENCE AT TOP.
- 19. CONCRETE DRIVEWAY ENTRANCE PER CITY STANDARD.



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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

NAME: JOSEPH T. RADACZ
SIGNATURE: _____
DATE: 03/24/26 LIC #: 45889

REVISIONS	ISSUE DATE: 03/24/26	BY: JTR
1.		
2.		
3.		
4.		
5.		
6.		
7.		

OWNER

VENTURE PASS PARTNERS, LLC
19620 Waterford Court
Shorewood, MN 55331

SITE PLAN

CHIPOTLE RESTAURANT
1909 - 1921 London Road
Duluth, MN 55811

SHEET NUMBER
C2.1

THE CONTRACTOR SHALL:
 VERIFY THE LAYOUT AND ANY DIMENSIONS SHOWN AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT ANY DISCREPANCIES THAT WOULD COMPROMISE THE DESIGN OR INTENT OF THE PROJECT LAYOUT.

PROTECT ALL EXISTING FEATURES FROM DAMAGE DURING PLANTING OPERATIONS. ANY DAMAGE SHALL BE REPAIRED OR REPLACED AT NO COST TO THE OWNER.

CALL GOPHER-ONE TO VERIFY ALIGNMENT AND LOCATION OF ALL UNDERGROUND AND ABOVE GRADE UTILITIES AND PROVIDE PROTECTION FOR SAME BEFORE CONSTRUCTION BEGINS.

PLANT MATERIALS:
 ALL PLANT MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE NURSERY STOCK STANDARDS AS PUBLISHED BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION (ANLA).

PLANTS SHALL NOT BE STOCKPILED ON SITE ANY LONGER THAN NECESSARY AND SHALL HAVE ADEQUATE WATERING AT ALL TIMES PRIOR TO PLANTING. PLANTS THAT SHOW OBVIOUS SIGNS OF DISTRESS FROM HEAT, OR LACK OF WATER, SHALL NOT BE INSTALLED. ROOT BALLS FOR ALL TREES SHALL BE COVERED WITH MULCH WHILE AWAITING PLANTING.

UNLESS NOTED OTHERWISE, DECIDUOUS SHRUBS SHALL HAVE AT LEAST 5 CANES AT THE SPECIFIED SHRUB HEIGHT. ORNAMENTAL TREES SHALL HAVE NO "V" CROTCHES AND SHALL BEGIN BRANCHING NO LOWER THAN 3' ABOVE THE ROOT FLARE. STREET AND BOULEVARD TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE THE ROOT FLARE.

THE LANDSCAPE PLAN TAKES PRECEDENCE OVER THE PLANT SCHEDULE IF ANY DISCREPANCIES IN QUANTITIES EXIST. SPECIFICATIONS TAKE PRECEDENCE OVER NOTES.

NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS REQUESTED OF THE LANDSCAPE ARCHITECT PRIOR TO THE SUBMISSION OF A BID AND/OR QUOTATION.

ADJUSTMENTS IN LOCATION OF PROPOSED PLANT MATERIALS ARE OCCASIONALLY NEEDED IN THE FIELD. SHOULD AN ADJUSTMENT BE REQUIRED, THE LANDSCAPE ARCHITECT MUST BE NOTIFIED IN ADVANCE TO DISCUSS AN ACCEPTABLE MODIFICATION.

ALL PLANT MATERIALS SHALL BE FERTILIZED UPON INSTALLATION WITH DRIED BONE MEAL. OTHER APPROVED FERTILIZER MIXED IN WITH THE PLANTING SOIL PER THE MANUFACTURER'S INSTRUCTIONS OR MAY BE TREATED FOR SUMMER AND FALL INSTALLATION WITH AN APPLICATION OF GRANULAR 0-20-20 OF 12 OZ PER 2.5" CALIPER PER TREE AND 6 OZ PER SHRUB WITH AN ADDITIONAL APPLICATION OF 10-10-10 THE FOLLOWING SPRING IN THE TREE SAUCER.

ALL PLANTING AREAS RECEIVING GROUND COVER, PERENNIALS, ANNUALS, AND/OR VINES SHALL RECEIVE A MINIMUM OF 8" DEPTH OF PLANTING SOIL CONSISTING OF AT LEAST 45 PARTS TOPSOIL, 45 PARTS PEAT/COMPOST AND 10 PARTS SAND.

IF THE LANDSCAPE CONTRACTOR IS CONCERNED OR PERCEIVES ANY DEFICIENCIES IN THE PLANT SELECTIONS, SOIL CONDITIONS OR ANY OTHER SITE CONDITION WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR GUARANTEE, HE MUST BRING THESE DEFICIENCIES TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO PROCUREMENT AND/OR INSTALLATION.

EDGING AND MAINTENANCE STRIPS:

BLACK POWDER COATED STEEL EDGER TO BE USED TO CONTAIN SHRUBS, PERENNIALS, AND ANNUALS WHERE BED MEETS SOD/SEED UNLESS NOTED OTHERWISE.

PLANTING NOTES:

TREES SHALL BE PLUMB IN BOTH DIRECTIONS AFTER PLANTING. STAKING IS SUGGESTED, BUT IS AT THE CONTRACTOR'S DISCRETION. THE TREE MUST BE MAINTAIN PLUMB THROUGH THE WARRANTY PERIOD AND CONFORM WITH A.N.A. GUIDELINES FOR STANDARD LANDSCAPE PRACTICES

CONIFEROUS TREES GROWN FOR COMMERCIAL CHRISTMAS TREE PRODUCTION CANNOT BE USED FOR THIS PROJECT. TREE SPECIMENS SHOULD HAVE NATURAL FORMS OTHER THAN LIMITED PRUNING FOR HEALTHY GROWTH AND PLANT STRUCTURE.

PRUNE ANY DAMAGED OR CROSSING BRANCHES REMOVE ALL FLAGGING AND LABELING AND DISPOSE FROM SITE

INSTALL BACKFILL IN 8-12" DEEP LAYERS & SATURATE SOIL WITH WATER. DO NOT COMPACT MORE THAN NEEDED TO MAINTAIN PLUMB

BACKFILL SHALL FORM WATERING RING AT OUTER EDGE AND BE FILLED WITH WATER AT TIME OF INITIAL PLANTING PRIOR TO MULCHING

VERIFY ROOT FLARE ELEVATION AND PLACE AT OR JUST ABOVE SURROUNDING GRADE.

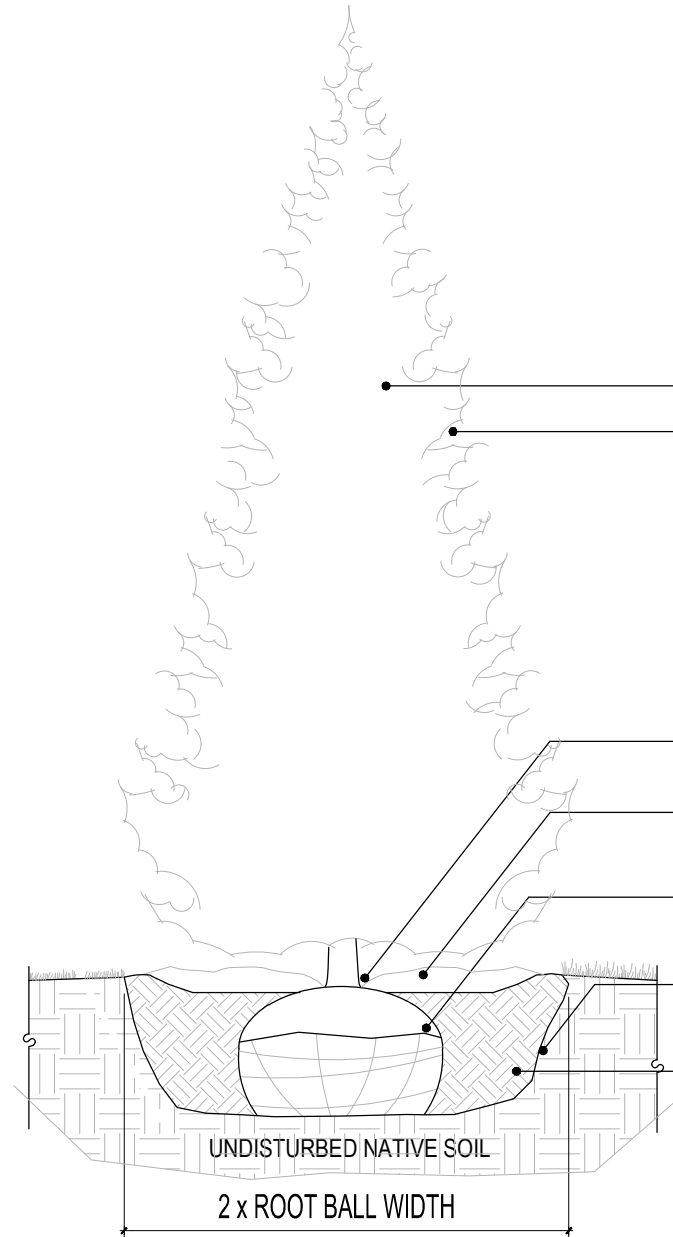
4" DEEP MULCH - NO MULCH IN DIRECT CONTACT WITH TRUNK

CUT BACK WIRE BASKET AND REMOVE BURLAP FROM TOP OF BALL

SCARIFY BOTTOM AND SIDES OF HOLE

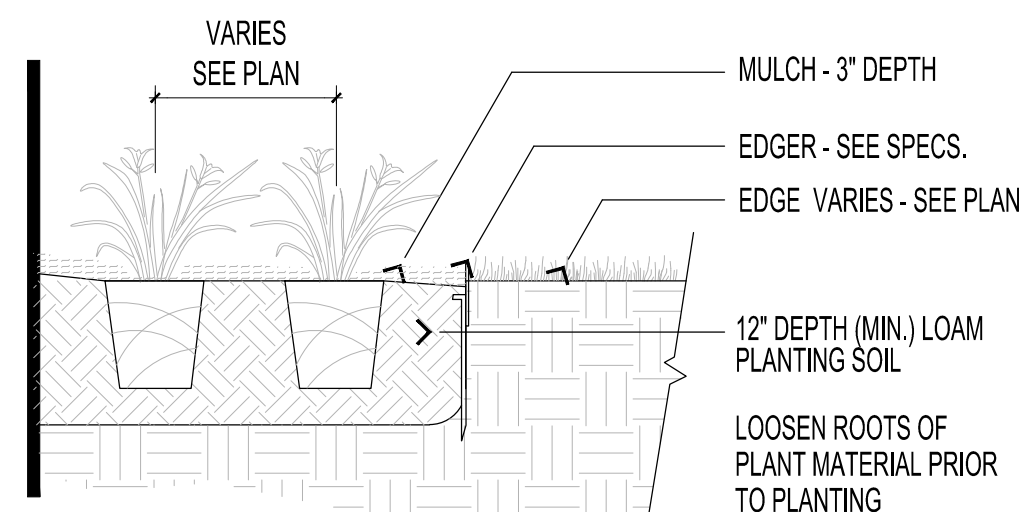
LOAM PLANTING SOIL

CONTRACTOR SHALL FIELD TEST PERCOLATION RATES PRIOR TO PLANTING AND NOTIFY LANDSCAPE ARCHITECT IF POOR DRAINAGE IS SUSPECTED



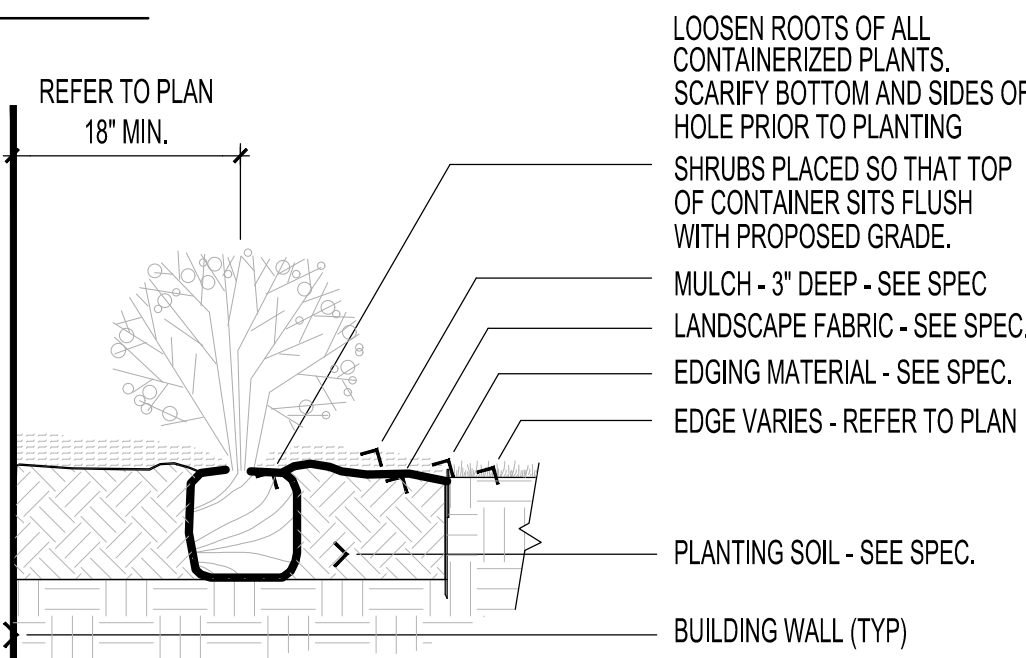
1 CONIFEROUS TREE PLANTING DETAIL

SCALE: 1/2" = 1'-0"



2 PERENNIAL PLANTING

SCALE: 3/4" = 1'-0"



3 SHRUB PLANTING DETAIL

SCALE: 3/4" = 1'-0"

MULCHING:

ROCK MULCH OR COBBLE SHALL BE CLEAN AND FREE OF ANY SIGNIFICANT DIRT, SOIL, OR ORGANIC MATTER THAT WILL PROMOTE WEED GROWTH. HIGH QUALITY GEOTEXTILE FABRIC, WITH EDGES OVERLAPPED AT LEAST 12", SHALL BE PLACED UNDER ALL ROCK MULCH.

ALL TREES SHALL HAVE A MULCH RING (MINIMUM 48" DIAMETER) OF 4" DEEP SHREDDED HARDWOOD MULCH WITH NONE IN DIRECT CONTACT WITH TREE TRUNK.

SPREAD GRANULAR PRE EMERGENT HERBICIDE (PREEN OR EQUAL) PER MANUFACTURER'S RECOMMENDED RATES UNDER ALL MULCHED AREAS INCLUDING TREE MULCH RINGS.

IRRIGATION:

VERIFY EXISTING/PROPOSED IRRIGATION SYSTEM LAYOUT AND CONFIRM LIMITS OF IRRIGATION PRIOR TO SUPPLYING SHOP DRAWINGS.

UNLESS SPECIFICALLY NOTED OTHERWISE, THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AN IRRIGATION LAYOUT PLAN AND SHOP DRAWING AS A PART OF THE SCOPE OF WORK WHEN BIDDING. THE IRRIGATION SHOP DRAWINGS SHALL BE REVIEWED BY THE LANDSCAPE ARCHITECT PRIOR TO ORDERING MATERIALS OR INSTALLATION.

THE LANDSCAPE CONTRACTOR SHALL INSURE THAT ALL SODDED/SEEDED AND PLANTED AREAS ARE IRRIGATED PROPERLY, INCLUDING THOSE AREAS DIRECTLY AROUND AND ABUTTING BUILDING FOUNDATION.

TURF OR OTHER SEEDED AREAS SHALL BE ON DIFFERENT ZONES THAN PLANTING BEDS IN ORDER TO CONTROL WATER FLOW TO DIFFERING PLANT SPECIES.

IRRIGATION TRENCHES SHALL BE ADEQUATELY COMPACTED TO AVOID SETTLEMENT IN THE FUTURE.

THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER WITH A WATERING/LAWN IRRIGATION SCHEDULE APPROPRIATE TO THE PROJECT SITE CONDITIONS AND TO PLANT MATERIAL GROWTH REQUIREMENTS.

SODDING:

WHERE SOD OR SEED ABUTS A PAVED SURFACE, FINISHED GRADE SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC.

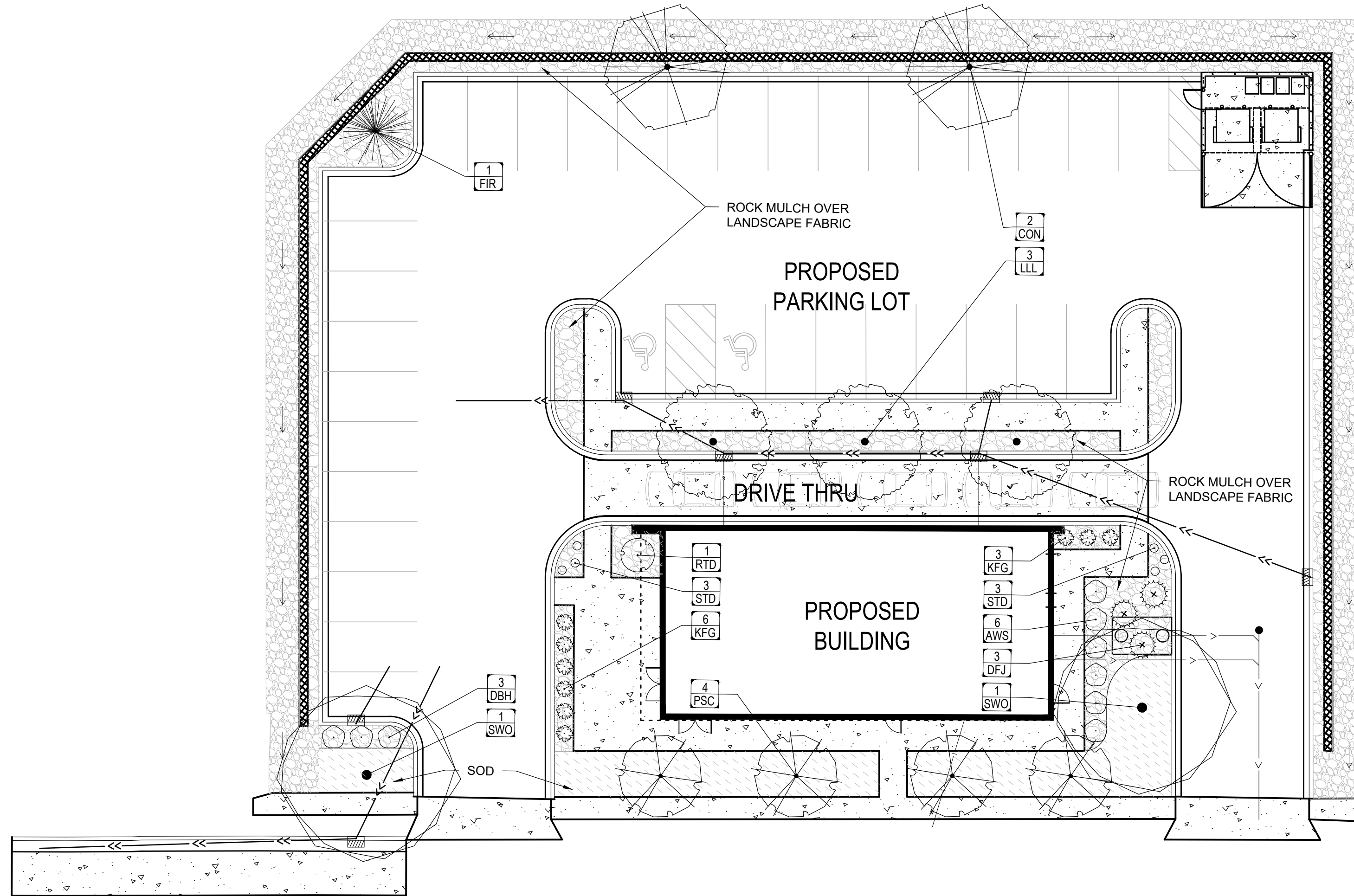
SOD SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. SOD MUST BE STAKED WITH WOOD LATH OR METAL STAKES ON SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES.

CALENDAR OR CLIMATIC RESTRICTIONS:

THE PREFERRED SODDING WINDOW IS FROM AUGUST 15th - OCTOBER 15th. HOWEVER, SOD MAY BE INSTALLED AT ANY TIME PROVIDED ADEQUATE IRRIGATION COVERAGE IS AVAILABLE. IF NO IRRIGATION IS AVAILABLE, THE CONTRACTOR IS RESPONSIBLE FOR SUPPLEMENTAL WATERING AS NEEDED FOR SOD ESTABLISHMENT.

LANDSCAPE PLANTING SHOULD GENERALLY OCCUR FROM MAY 1st - JUNE 15th OR FROM SEPTEMBER 1st - OCTOBER 15th. PLANTING OUTSIDE THESE DATES IS NOT RECOMMENDED, BUT CAN OCCUR WITH PROPER CARE OF NURSERY STOCK ON SITE AND WITH AMPLE WATERING.

PLANTING SHALL NOT OCCUR IF TEMPERATURES ARE HIGHER THAN 85 F. DEGREES, OR IF WINDS EXCEED 15 MPH DURING PLANTING OPERATIONS.



OVERALL TREE PLANTING & LANDSCAPE PLAN:

GRAPHIC	MATERIAL	SPECIFICATION	NOTES
[Rock Mulch Symbol]	ROCK MULCH	1.5" DIAMETER CRUSHED GRANITE ROCK MULCH	3" DEPTH
[Sod Symbol]	SOD	BLUEGRASS PEAT SOD	N/A

GROUND COVER SCHEDULE:

SYM	QTY	COMMON NAME	SCIENTIFIC NAME	SIZE	CONT	COMMENTS
DECIDUOUS TREES						
CON	2	SUGAR CONE MAPLE	Acer saccharum 'Sugar Cone'	2.5" CAL	B&B	FULL FORM
LLL	3	LITTLELEAF LINDEN	Tilia cordata	2.5" CAL	B&B	FULL FORM
PSC	4	PINK SPIRES CRABAPPLE	Malus x 'Pink Spires'	1.5" CAL	B&B	FULL FORM
SWO	2	SWAMP WHITE OAK	Quercus bicolor	2.5" CAL	B&B	FULL FORM
EVERGREEN TREES						
FIR	1	CONCOLOR FIR	Abies concolor	5' HGT	B&B	FULL FORM
DECIDUOUS SHRUBS						
AWS	6	ANTHONY WATERER SPIREA	Spiraea x bumalda 'Anthony Waterer'	24" HGT	POT	4' O.C.
DBH	3	DWARF BUSH HONEYSUCKLE	Diervilla lonicera	24" HGT	POT	4' O.C.
RTD	1	FIRE DANCE RED TWIG DOGWOOD	Cornus sericea 'Bailadeline'	24" HGT	POT	8' O.C.
CONIFEROUS SHRUBS						
DFJ	3	DAUB'S FROSTED GOLD JUNIPER	Juniperus x chinensis 'Daub's Frosted'	3 GAL	POT	5' O.C.
PERENNIALS & ORNAMENTAL GRASSES						
KFG	9	KARL FOERSTER GRASS	Calamagrostis x acutiflora 'Karl Foerster'	2 GAL	POT	PER PLAN
STD	6	STELLA d' ORO DAYLILY	Hemerocallis x 'Stella d' Oro'	1 GAL	POT	PER PLAN

PLANTING SCHEDULE (THIS PAGE ONLY):

1. THE CITY OF DULUTH LANDSCAPE ORDINANCE REQUIRES DIFFERENT LANDSCAPE TREATMENTS AS FOLLOWS:

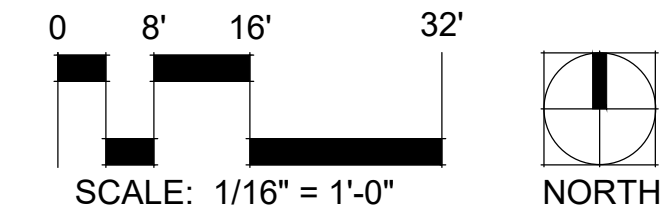
2. STREET FRONTAGE - 197 LF ALONG LONDON ROAD
 1 TREE PER 35 LF OF FRONTAGE = 6 TREES REQUIRED
 1 SHRUB PER 25 LF OF FRONTAGE = 8 TREES REQUIRED

3. INTERNAL LANDSCAPING AREA FOR PARKING AREAS - 1,504 SF
 1 TREE PER 300 SF OF LANDSCAPED AREA = 5 TREES REQUIRED

4. LANDSCAPING HAS BEEN PROVIDED AS FOLLOWS:
 • STREET FRONTAGE OVERSTORY TREES = 2 TREES
 • STREET FRONTAGE ORNAMENTAL TREES = 4 TREES
 • INTERIOR OVERSTORY TREES = 5 TREES
 • INTERIOR CONIFEROUS TREES = 1 TREE
 • DECIDUOUS SHRUB PLANTINGS = 10
 • CONIFEROUS SHRUB PLANTINGS = 3
 • ORNAMENTAL GRASS PLANTINGS = 9 TOTAL
 • PERENNIAL PLANTINGS = 6 TOTAL

3. PROPOSED MINIMUM SIZES ARE:
 • 2.5" CAL. FOR DECIDUOUS TREES
 • 5' HEIGHT FOR CONIFEROUS TREES
 • 24" HEIGHT FOR DECIDUOUS SHRUBS
 • 18" HEIGHT FOR EVERGREEN SHRUBS

LANDSCAPE CALCULATIONS:



14165 James Road - Suite 200A
 Rogers, MN 55374

Phone: 612-237-8355
 www.insideoutsidearchitecture.com

Project Name:
CHIPOTLE RESTAURANTS

Duluth, Minnesota

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Name: Paul Kangas
 Registration #: 26017

Signature: Paul Kangas

Project #: _____
 Date: CITY SUBMITTAL | 03-24-2026
 Revision: _____
 Drawn By: PK
 Checked By: PK

Sheet Title:
LANDSCAPE PLAN

Sheet Number:

L1-1

Interim Use Permit Request Narrative

The applicant is requesting that the city grant an Interim Use Permit for the operation of a Chipotle Mexican Grill consistent with its national standards. We believe the project can comply with all the principles of the Neighborhood Mixed Use guide plan designation, and the F-2 zoning requirements, except for the maximum parking standard which caps the parking at 24 spaces for the proposed 2379 square foot restaurant. We are requesting that 11 additional parking spaces along the 19th Avenue end of the property be allowed as an interim use so long as Chipotle Mexican Grill operates on the property.

While Chipotle is sympathetic with the city's goals of minimizing impervious surfaces and utilizing alternative transportation, 24 spaces will not accommodate their customers' needs. Chipotle has grown to operate over 4,000 restaurants and is widely viewed as one of the most successful fast/casual restaurant companies in the nation. During this time, they have developed analytical tools that allow them to very accurately model their guest traffic, and overall performance of their new store locations. London Road is expected to be a high-volume location. Chipotle has also learned by experience that inadequate parking negatively impacts the guest experience, limits sales during peak hours, and can cause unsafe congestion within the parking lots, which in this case could spill onto London Road.

The existing Chipotle location at Miller Hill Mall provides a good reference for customer traffic. This store averages 76 transactions per hour during the period from noon to 1:00 PM and regularly achieves 90 or greater transactions within the hour. This demand, coupled with the parking requirements of Chipotle's employees that total 12-13 when fully staffed, evidences the need for the 11 additional parking stalls that are being requested.

The Chipotle will reduce some parking requirements, but the vast majority of Chipotle customers still prefer the in-store experience. The limited on-street parking and bus service is expected to have only a minimal impact on the on-site parking requirements. While Chipotle expects to be a welcome destination to the immediate neighborhood, only a very small percentage of their overall customer base is expected to walk or bike to this location.

We respectfully request that the city consider the needs and judgement of Chipotle by accommodating our request for an Interim Use Permit.

Statement about how the requested use will not have a negative impact on the community

The proposed redevelopment of the residential properties at 1913 - 1922 London Road into a Chipotle restaurant is not expected to have a negative impact on the surrounding community. Instead, the project represents a reinvestment in an aging site and will provide a neighborhood-serving commercial use that is compatible with the evolving character of the London Road corridor.

Compatibility with Existing Corridor Development

London Road functions as a major transportation corridor and already supports a mix of commercial, service, and residential uses. Redeveloping the residential properties for restaurant use aligns with the corridor's existing development pattern and does not introduce a land use that is inconsistent with surrounding properties.

Restaurants are common neighborhood-serving uses along arterial corridors and provide services for nearby residents, workers, and visitors. The proposed development will replace two aging residential structures built in the 1920s with a modern commercial building that is designed to meet current building codes, accessibility standards, and site design requirements.

Limited Traffic and Short Duration Visits

Chipotle operates within the **fast-casual restaurant category**, which is characterized by quick service and short customer visits. Typical visits are brief and designed for efficient service, often lasting only a few minutes for ordering and pickup. Chipotle Mexican Grill restaurants are designed for high throughput and fast service, with many customers entering and exiting within a short timeframe.

Because of this operational model, fast-casual restaurants generally produce **steady but moderate traffic flows rather than prolonged congestion**, and customers typically remain on site for a limited time compared with traditional sit-down restaurants. This reduces the likelihood of extended parking occupancy or lingering activity that could negatively affect nearby properties.

Site Design and Traffic Management

The redevelopment will incorporate a modern site layout designed to manage traffic flow safely and efficiently. Typical site planning elements for restaurants include:

- Clearly defined entry and exit points

- Adequate off-street parking
- Internal circulation designed to avoid conflicts between vehicles and pedestrians
- Designated pickup areas
- In App Purchases are done prior to vehicles coming to the site (no ordering board)
- Bike friendly access and parking/bike racks

These design elements help ensure that traffic generated by the restaurant is accommodated within the site rather than affecting adjacent streets or nearby residential areas.

Improvement of Site Conditions

The existing properties contain residential structures that are approaching 100 years in age and were originally constructed for a residential neighborhood that has changed significantly over time. Redeveloping the site will:

- Replace older structures with modern construction
- Improve landscaping and site maintenance
- Provide upgraded lighting and visibility
- Enhance the overall appearance of a highly visible corridor property

These improvements will contribute positively to the surrounding streetscape and neighborhood environment.

Economic and Community Benefits

The project will provide several positive benefits to the community, including:

- Creation of local employment opportunities
- Increased commercial activity along London Road
- Additional dining options for nearby residents and visitors
- Increased property tax revenue and economic investment in the area

Well-recognized national brands can also act as neighborhood anchors that attract visitors and contribute to nearby commercial activity.

Conclusion

The proposed Chipotle restaurant at 1913 - 1922 London Road will not have a negative impact on the surrounding community. The development is compatible with the existing corridor context, generates manageable traffic typical of fast-casual restaurants, and improves the site through modern design and investment.

Overall, the project represents a responsible redevelopment that enhances the corridor while providing a useful service to the surrounding neighborhood.

Project consistency with comprehensive plan 12 guiding principles

Reuse Previously Developed Lands

The project directly aligns with the principle of reusing previously developed lands, which encourages redevelopment of existing properties to avoid unnecessary expansion into undeveloped areas. The city emphasizes that redevelopment of existing structures or sites “directs new investment to sites which have the potential to perform at a higher level than their current state.”

The two subject parcels are already fully developed residential lots with existing utilities, roadway access, and infrastructure. Redeveloping these sites for a restaurant:

- Utilizes existing infrastructure including streets, utilities, and municipal services
- Eliminates the need to extend development into undeveloped land
- Represents an efficient reinvestment into underutilized urban property

By repurposing previously developed land along a major transportation corridor, the project supports the City’s goal of directing growth toward locations where infrastructure already exists.

Strengthening Neighborhoods

The project supports the principle of strengthening neighborhoods by investing in a highly visible corridor and providing a neighborhood-serving commercial use.

The existing houses were constructed nearly a century ago and were designed for a residential context that has evolved significantly over time. London Road now functions as a major transportation corridor and mixed-use area with commercial, service, and residential uses. Redeveloping these parcels for a modern restaurant:

- Activates a prominent corner site
- Improves property conditions through new construction and landscaping
- Provides a local affordable dining option within walking, biking and driving distance of nearby residents

This reinvestment helps maintain the vitality and safety of the corridor while supporting the surrounding neighborhood.

Reinforcing the Place-Specific Character of the Corridor

The governing principles encourage development that reinforces the place-specific characteristics of areas within the city. London Road serves as an important east-west corridor connecting neighborhoods along Lake Superior and supporting a mix of neighborhood-scale commercial uses.

The proposed Chipotle restaurant is consistent with this context because:

- It provides a neighborhood-serving commercial use appropriate for a corridor street
- The building and site design will incorporate landscaping, pedestrian access, and site improvements consistent with corridor development patterns
- The project enhances the visual character of the area compared to aging residential structures that are no longer aligned with the corridor's evolving land-use pattern.

Encouraging a Mix of Activities, Uses, and Densities

Cities function best when they support a mix of land uses and activities, rather than strictly segregated development patterns. The governing principles note that mixed uses allow for greater diversity of activity than uniform land uses.

This project contributes to that goal by introducing a neighborhood commercial use within an area that already includes residential, institutional, and commercial activity. The addition of a restaurant:

- Provides a service to nearby residents and employees
- Encourages local activity along the corridor
- Supports a balanced mix of uses within the neighborhood.

Supporting Private Investment that Contributes to the Public Realm

Private development should enhance the surrounding public environment through thoughtful site and building design. The Duluth principles emphasize that building form, setbacks, lighting, and site design can positively influence nearby public areas.

The proposed redevelopment will contribute positively to the public realm through:

- Modern building architecture and landscaping
- Improved site circulation and parking organization

- Pedestrian access and connectivity to the sidewalk network
- Enhanced lighting and visibility along the corridor

These improvements will create a safer, more attractive streetscape compared to the current condition of the site.

Supporting Economic Development

The project also aligns with the principle of supporting the city's economic base and emerging economic growth sectors. Restaurant and service businesses contribute to local employment opportunities, sales tax revenue, and increased commercial activity.

The Chipotle restaurant will:

- Create new employment opportunities
- Generate economic activity along the London Road corridor
- Provides a highly successful national brand that attracts visitors and residents alike.

Sustainable Use of Existing Infrastructure

The redevelopment supports the principle of taking sustainable actions by making efficient use of existing infrastructure. Because the site already has roadway access, municipal utilities, and transit availability along London Road, the project minimizes the need for new infrastructure investment and supports efficient delivery of public services.

Summary

Redeveloping the properties at 1913 - 1922 London Road into a Chipotle restaurant is consistent with Duluth's governing planning principles. The project:

- Reuses previously developed land
- Strengthens the surrounding neighborhood
- Reinforces the evolving character of the London Road corridor
- Encourages a mix of uses and activities

- Enhances the public realm through improved site design
- Supports economic development and job creation
- Efficiently utilizes existing infrastructure and public services

Through these contributions, the project represents a responsible community and supports the long-term planning goals established reinvestment in the by the City of Duluth.

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